

PARKING REGULATIONS REPORT

Town of Wellesley, Massachusetts



July 2014

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With selective edits by the Planning Board and Planning Department

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1.0 BACKGROUND AND INTRODUCTION

Parking is one of the most sensitive issues in the development and site planning process. Area residents often see parking as their right, while local merchants look at it as their economic lifeline. At the same time, opening up large areas for new parking is often very difficult given the cost of land acquisition, or community opposition to loss of open space. These factors ensure that the provision of parking that works for everyone remains a primary challenge for municipalities.

In the spring of 2013, the Wellesley Planning Board engaged Howard/Stein-Hudson Associates to develop a new Off-Street Parking Zoning Bylaw for the Town. The objectives of this project are to:

- Analyze the existing zoning provisions;
- Analyze and educate the Board on current off-street parking best practices;
- Assess innovative zoning techniques relative to the current developable land constraints and business growth in the commercial districts;
- Identify feasible incentives and special permit provisions; and
- Produce off-street parking zoning language and implementation strategies.

The Wellesley Zoning Bylaw has included off-street parking requirements since 1950. While there have been many revisions over the years, the current regulations reflect the 1971 amendments, which require a number of parking spaces based on the underlying zoning district and use and square footage of the building(s). This has been the dominant standard for over 40 years and continues to the present day. Confusing and stringent grandfathering provisions further constrain the Town from improving parking as redevelopment projects have come forward. In 2007, the Board attempted unsuccessfully to modify the parking requirements to a “use-based” table; this failure was in large part due to perceived potential impacts of the changes on the proposed Wellesley High School project. Generating consensus on potential modifications to these existing grandfathering provisions has been a challenge.

The highest priority and top recommendation in this report is to adopt a new parking bylaw, which provides clarity, flexibility, efficient land use, context-based parking placement, sustainable design standards, and encourages access to development by pedestrian, bicycle, and transit facilities as well as vehicles.

Through this study, the Planning Board is seeking innovative alternatives to both the existing off-street parking requirements and development standards for parking lots and facilities. Alternative parking standards recommended in this study take into consideration Wellesley's distinct historic settlement patterns and characteristics. This includes the vehicular traffic and parking demands that are generated by local residents, as well as by the many students, tourists, commuters and visitors. The Board understands that achieving consensus among commercial property owners, merchants, and other stakeholders on any significant changes to parking regulations and policies may be a challenge.

As important as off-street parking regulations are, other best practices must also be embraced, such as providing and managing public parking in a sustainable manner; maximizing parking efficiency through shared use and demand management; utilizing up-to-date meter technology; varying meter time limits to respond to differing parking needs; and implementing shared parking before attempting to create new parking. If additional parking is deemed necessary and appropriate, it must be integrated with its surroundings through the use of environmentally sensitive landscaping and low impact development techniques. In evaluating the parking challenges and opportunities in Wellesley, this Study considers the following issues and potential future strategies:

- Modifying requirements to accommodate mixed-uses within certain districts;
- Managing parking supply through the use of shared-parking to reduce overall supply requirements;
- Determining the feasibility of parking overlay in certain districts where such shared-parking would be allowed;
- Determining locations where added structured parking might be appropriate;
- Determining how effectively time limits on parking meters allow for proper turnover of short-term spaces for customers, and long-term spaces for employees;

- Determining how well all-day commuter parking is being directed to sites where it does not conflict with short-term shopper parking;
- Accommodating bicycle parking, especially in the village core areas; and
- Developing the potential for sustainable paving and landscaping techniques for new or retrofitted lots.

2.0 PARKING CHALLENGES AND OPPORTUNITIES

2.1. GENERAL CONDITIONS AND TRENDS

Community Settlement Patterns and Development Characteristics

The Town of Wellesley is approximately 10 miles west of Boston, and is bordered by Natick on the west; Weston and Newton on the north and northeast; Needham on the southeast; and Dover on the southwest. The Town's settlement pattern reflects a series of predominantly single-family neighborhoods connected by major corridors, village centers, and institutions. Wellesley's commercial districts and village centers are primarily located along or near the Town's two major east-west arterial roads: Washington Street (Routes 135 and 16), and Worcester Street (Route 9). There are four village-style shopping areas: Lower Falls, Wellesley Hills, Wellesley Square, and Linden Square. In addition to major arterial roads, Wellesley Square and Wellesley Hills are well served by the Framingham/Worcester MBTA commuter rail line; additionally, Linden Square is in close proximity to the Wellesley Square station, and Lower Falls is reasonably accessible from the Wellesley Farms station. There are three other neighborhood commercial areas of retail, service, and auto-oriented uses, located along Cedar Street, the Fells (Weston Road), and the State Street area. Finally, along the Newton town line along Worcester Street (Route 9), Wellesley has a strong office presence with developments along Walnut Street, as well as Sunlife, Harvard Pilgrim, and the Wellesley Office Park; and on the Natick town line, there are office, service and retail uses. Collectively, these districts generate significant traffic and parking demand.

Relevant Plans, Reports and Studies

A summary evaluation of local resources related to community development issues, such as parking, includes the following:

- Wellesley Comprehensive Plan, 2007-2017;
- Wellesley Zoning Bylaw;
- Wellesley Design Guidelines;
- Resident Attitude Study March, 2004;
- Linden Street Vision Plan, 2004;
- Wellesley Square Action Plan;
- Wellesley Walks Pedestrian Plan;
- Lower Falls Zoning, Urban Design, and Landscaping Guidelines;
- State Street Area Plan; and
- Wellesley Hills Square Study

In 2006, the Planning Board completed the *Town-wide Comprehensive Plan 2007-2017* which recommends the management of parking to support commercial districts. However, the current parking zoning regulations do not allow for shared parking, valet parking, or provisions for delivery service. Considerable attention has been paid to parking issues over the years through various plans and studies. The Town is currently prepared to substantially update and revise off-street parking regulations, in order to reflect the existing and future demands in the established village centers, neighborhood districts, and commercial corridors.

2.2. DISTRICT PARKING INVENTORY AND EVALUATION

Howard/Stein-Hudson Associates (HSH) conducted field observations of the off-street and on-street parking supplies (both public and private) of the 5 targeted parking districts: Wellesley Square, Lower Falls, Linden Square, State Street, and Wellesley Hills. Data evaluation included on-site observations of access, capacity, occupancy, turnover, and distribution. HSH then compared the existing parking characteristics to the current parking bylaw requirements; the potential reuse, rehabilitation, and redevelopment in the districts based on use and dimensional standards; and general market opportunities.

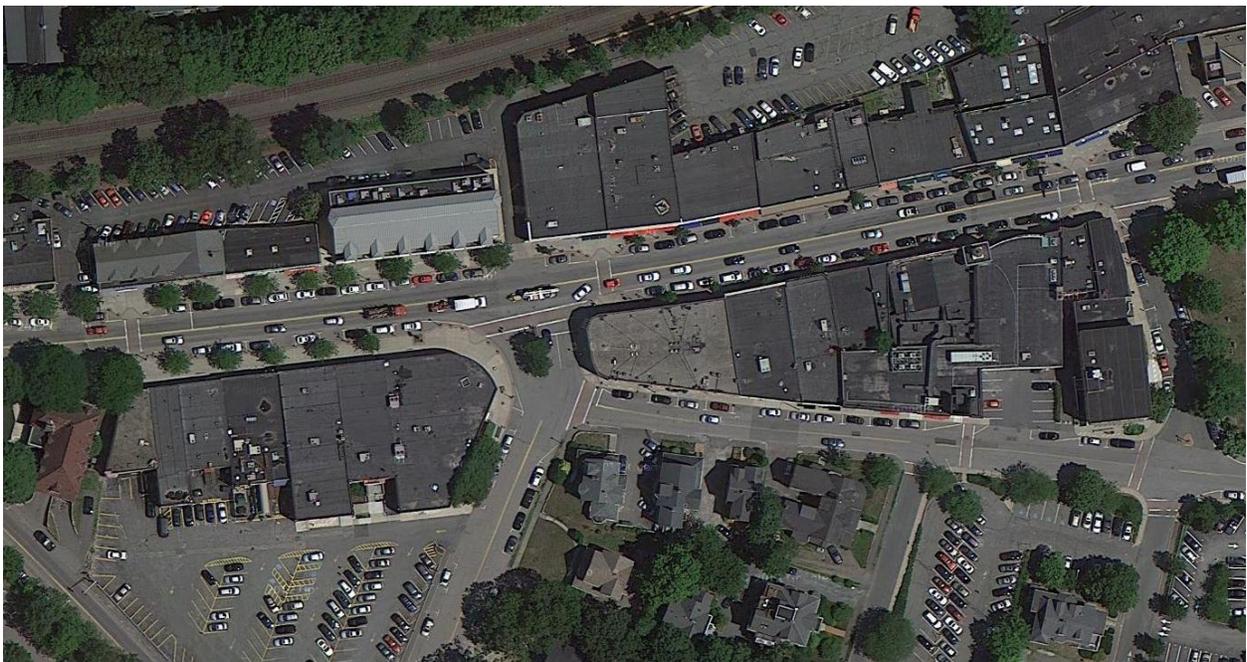
Wellesley Square

Wellesley Square has numerous opportunities for parking requirement reduction. The area has, by far, the most public parking of all of the five targeted districts in Wellesley. This public parking includes four well-distributed public parking lots located in different areas of Wellesley Square. On-street parking is also available on almost every street in the square, which is ideal for short-term, one-stop trips. There are also a large number of private parking lots, most of which are located behind businesses. A parking inventory for Wellesley Square is shown in **Table 1**.

Wellesley Square is very walkable. The pedestrian environment is enhanced by a large number of businesses that are located within relatively small footprints. These businesses have narrow frontages, attractive signs, window displays, and outdoor furnishings. As shown on the base map, most civic and commercial buildings are located within the first pedestrian precinct, less than a quarter mile from the center. A large number of homes are located within a comfortable walking distance of Wellesley Square, allowing nearby residents to visit without the use of a car. Wellesley College, located at the western edge of the square, attracts and generates a large number of pedestrian trips. The Hunnewell Elementary School is also located within the square on Cameron Street; parents walking their children to school can easily walk to the neighborhood shops before heading home.

Land and building use in Wellesley Square is diverse and opens itself to shared parking opportunities. Businesses in the square are predominantly small shops, restaurants, cafes, and financial institutions with some larger offices. The 396,000 square feet of commercial space in Wellesley Square is divided between small retail uses (299,000 square feet, or 75%); mixed use buildings (86,000 square feet, or 21%); office space (6,650 square feet, or 2%); banks (2,530 square feet, or 1%); and eating/drinking places (2,000 square feet, or 1%). Residential uses include 9 condominium units at 594 Washington Street, two condominium units at 9 Cross Street, and a two-family home on Grove Street. The area around the Wellesley Square district is primarily residential. In terms of future development, the existing types of businesses will likely continue to thrive with a limited amount of new infill mixed use development. One development at 576 Washington Street (the former Wellesley Inn site) will include 30 dwelling units and approximately 9,500 square feet of retail, expected to include a restaurant.

Wellesley Square also has several public buildings which are not open on certain days of the week or times of the day. These include the Wellesley Free Library, Hunnewell Elementary School, the US Post Office, and Wellesley Town Hall. The Square's close proximity to Wellesley College may present opportunities for shared parking arrangements when parking demand at the college is light.



The Wellesley Square MBTA commuter rail station is located at the heart of Wellesley Square. Wellesley Square is a 40-minute ride from South Station on the

Framingham/Worcester Line. The station is within walking distance from a large number of homes and businesses, meaning that residents commuting to work could walk from their homes, and visitors can access the Square without the need for a car. There are no programs currently in place to provide preferential parking to Wellesley residents at Wellesley Square station. This type of program would encourage residents of Wellesley to use public transportation rather than rely on an automobile to get to work.

Wellesley Square is located just northwest of the Brook Path, a multi-use path that runs northeast/southwest through Wellesley. The Path's proximity to the Square attracts pedestrians, cyclists, and other non-motorized modes of transportation, some of whom may have come from miles away. The Town could consider a provision allowing for the reduction in the required number of parking spaces if bicycle parking is provided nearby.

Table 1. Wellesley Square Parking Inventory

Public Parking	2-Hour Spaces	4-Hour Spaces	10- Hour Spaces	Handicap Spaces	All-Day Spaces**	Total
Railroad Avenue Lot		55	16	5		76
Waban Street Lot	15	94		4		113
Cameron Street Lot	11	28	12	4	78	133
Tailby Lot at MBTA Station*				2	221	223
Lot Totals	26	177	28	15	299	545
Weston Road	3		11			14
Cross Street	23					23
Church Street	42					42
Grove Street	48			1		49
Washington Street	79	17	26			122
Central Street	80			2		82
Crest Road	4			1		5
Street Totals	279	17	37	4	0	337
Wellesley Square Public Totals	305	194	65	19	299	882

Private Lots**	General	Handicap	Total
Behind 74-102 Central Street	111	5	116
At church/#2 Central Street	10		10
Behind/adjacent to 8-32 Church Street	80	2	82
Behind 25-71 Central Street	66		66
Behind 73-103 Central Street	54	2	56
At 9 Central Street/Peet's	7		7
Post Office lot	29	2	31
Wellesley Town Hall Driveway	10		10
Wellesley Town Hall Driveway	20		20
Wellesley Town Hall Parking Lot	26	4	30
Private Totals	413	15	428

Wellesley Square Grand Totals	1310
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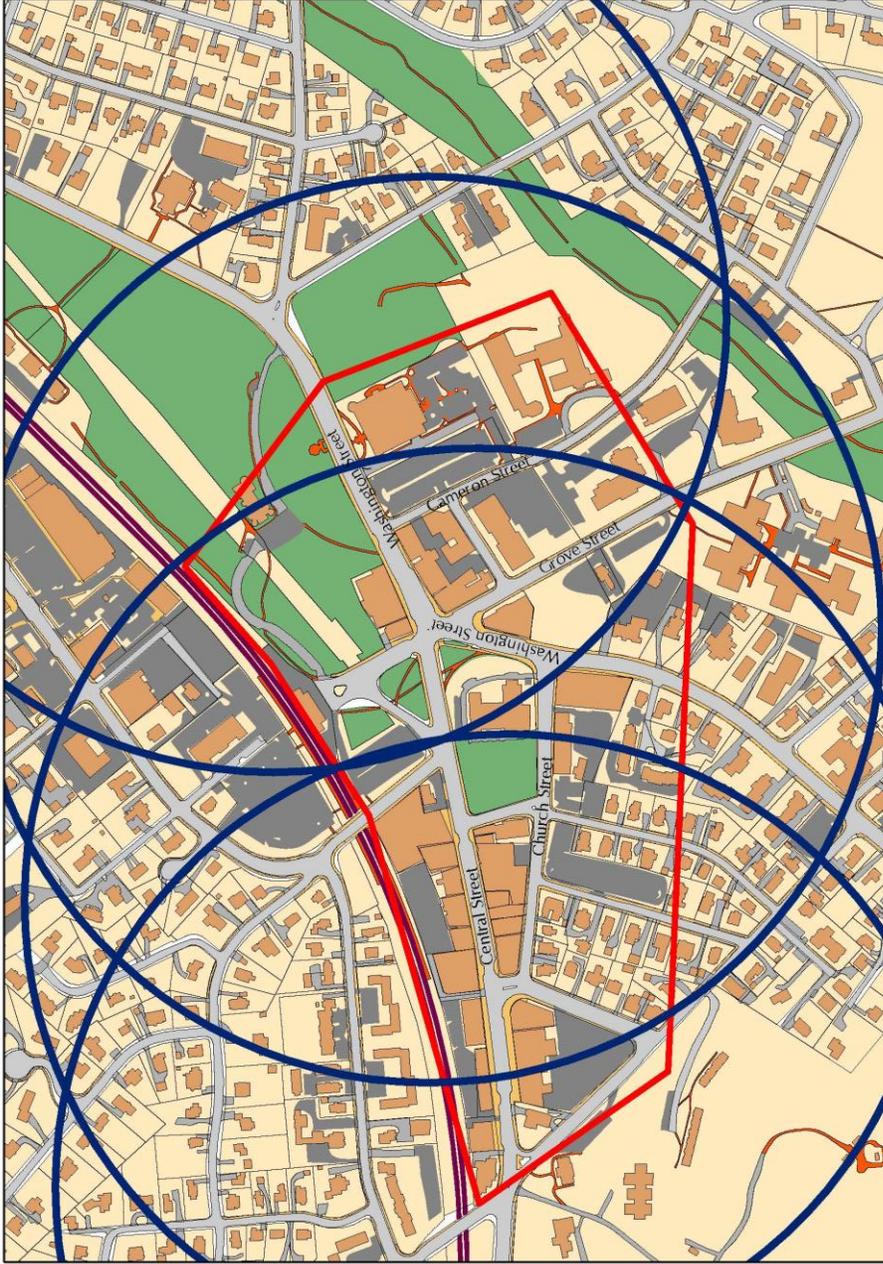
*Tailby Lot also considered to be within the Linden Square district.

**Based on "Long-Term" spaces from 2004 parking study and aerial imagery. Private lots based primarily on aerials.



Wellesley Square Base Map

- Legend**
- Wellesley Square Pedestrian Precincts
 - Wellesley Square Boundary
 - Buildings
 - Roadways
 - Parking
 - Framingham - Worcester Line
 - Sidewalks
 - Paths
 - Parks
 - Parcels



1 in = 300 feet

Linden Square

The Linden Street commercial area, also known as Linden Square, consists primarily of large commercial uses with large private parking lots between the buildings and the street. However, there are still opportunities for shared parking in this area. A parking inventory for Linden Street is shown in **Table 2**.

There is approximately 351,000 square feet of leasable commercial space in Linden Square. The majority of this space (237,000 square feet, or 68%) is classified as shopping centers/malls. Another 21% (75,200 square feet) is classified as general office space. The remaining land uses include small retail locations, hardware stores, and mixed-use buildings. Limited residential uses along Linden Street, within a comfortable walking distance to the Square, include a two-family home at 127 Linden Street; a small multi-unit residential building at 172 Linden Street; and nine condominium units, including two at 248-250 Linden Street, and seven at 278-290 Linden Street. Future development is anticipated to be small scale infill and reuse. (While the Volkswagen dealership was originally intended to be redeveloped consistent with the development of other portions of Linden Square, Town Meeting has approved an amendment to the original Development Agreement allowing the dealership to remain for twenty (20) years)



Current uses do not necessarily have the same peak period of use. On a typical Saturday, the peak hour of a fitness center may occur in the morning, the peak hour of a grocery store may occur during the afternoon, and the peak period of a restaurant may occur in the evening. The quantity of parking spaces demanded by the grocery store may easily accommodate adjacent uses during off-peak hours.

“Linden Square”, which is at the intersection of Linden Street and two major commercial driveways, is located within a ½-mile walk of the Wellesley Square MBTA commuter rail station. Visitors arriving by train would be able to walk to Linden Square from the train station to pick up groceries, meet a friend for dinner, etc. Wellesley Square station also has public parking available.

Linden Street is surrounded by residential neighborhoods, making it easy for nearby residents to walk to the corridor. It is also within a mile of Wellesley Square allowing visitors to easily walk between the two rather than driving.

Table 2. Linden Street Parking Inventory

Public Parking	2-Hour Spaces	Handicap Spaces	All-Day Spaces	Totals
Tailby Lot at MBTA Station*	0	2	221	223
Lot Totals	0	2	221	223
Linden Street	8			8
Street Totals	8	0	0	8
Linden Square	8	2	221	231

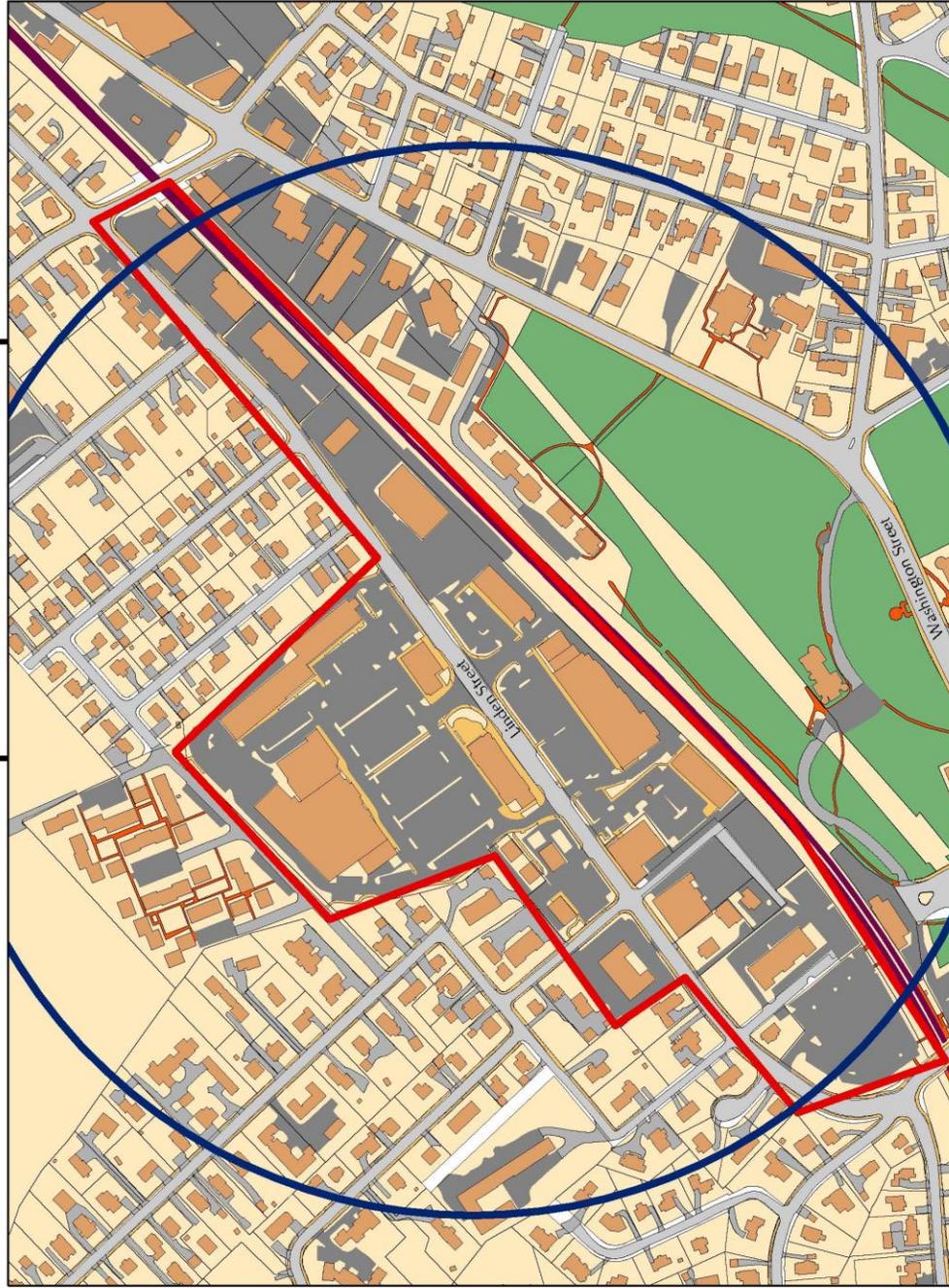
Private Lots**	General	Handicap	Total
Linden Square Plaza North	483	18	501
Linden Sq Plaza South plus 159 Linden	256	8	264
Citgo/162 Linden	42	2	44
148 Linden	74		74
Jack Payne Florist	128		128
One Hollis Street	119	3	122
Wellesley Volkswagen**	264		264
277 Linden	58	1	59
The Captain's Table	36		36
289 Linden	33		33
Private Totals	1493	32	1525

Linden Square Grand Totals	1756
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*Tailby Lot also considered to be within Wellesley Square district

**Based on # of spaces on lot. May be more appropriate to only consider customer/employee spaces.

Linden Square Base Map



- Legend**
- Linden Square Pedestrian Precincts
 - Linden Sq Boundary
 - Buildings
 - Roadways
 - Parking
 - Framingham - Worcester Line
 - Sidewalks
 - Paths
 - Parks
 - Parcels

1 in = 250 feet

State Street

State Street is a business district along Washington Street that is anchored by Whole Foods Market and athletic playing fields to the east, and the Wellesley Police Department to the west. The area also contains small retail shops, two banks, a daycare, and an automotive repair shop, among others.

There is some public on-street parking on the west side of the State Street study area, but most of the parking is located on individual properties. There is also a parking lot located on State Street that is owned by the Town. This area, which contains ## spaces, has been used for overflow parking or rented to local merchants. Public on-street parking is provided adjacent to the playing fields to the east of State Street. A parking inventory is shown in **Table 3**.



Land and building use in the State Street district is primarily commercial with a few residential uses. Approximately 93,800 square feet of rentable space is available in the State Street district. The primary commercial use in the State Street area is the Whole Foods Market located at 442 Washington Street, which accounts for 35% (33,900 square feet) of the commercial space in the district, is currently the biggest draw, and is likely the largest employer in this district. Other uses include small retail stores (23,700 square feet, or 26%); Haskins Automotive at 467 Washington Street (17,400 square feet, or 19%); and office space (11,500 square feet, or 12%). The primary commercial use is Whole Foods, which is the largest draw and employer in this district. In terms of future

development, infill may be possible at Haskins. There may be good potential for higher density mixed use given the visibility and proximity of the property to similar uses.

The State Street district (as identified in the Planning Department’s “State Street Study”, conducted in 2009) is located very close to residential neighborhoods. For residents of these neighborhoods, trips to some locations in the State Street area would not require a car. The State Street area is also located a little more than ½ mile from the Linden Street, Wellesley Hills, and Wellesley Square areas, making trips between these districts also possible without the use of a car.

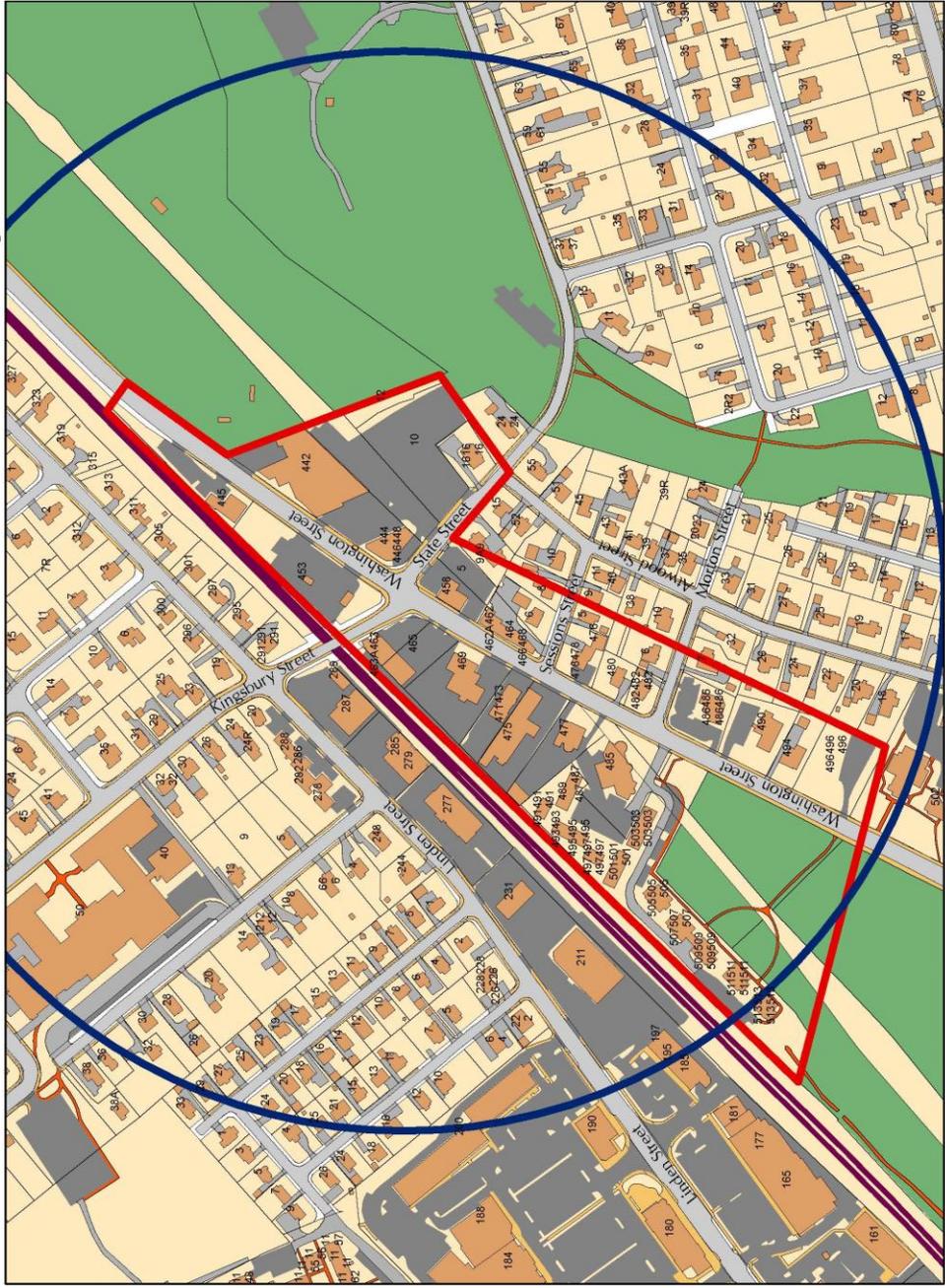
Table 3. State Street Area Parking Inventory

Public Parking	2-Hour Spaces	4-Hour Spaces	10-Hour Spaces	Handicap Spaces	All-Day Spaces*	Totals
Behind Needham Bank (ret. Wall)	-	-	-	-	28	28
Lot Totals	0	0	0	0	28	28
Washington Street (sub 1)					13	13
Washington Street (sub 2)					1	1
Street Totals	0	0	0	0	14	14
State Street Public Totals	0	0	0	0	42	42

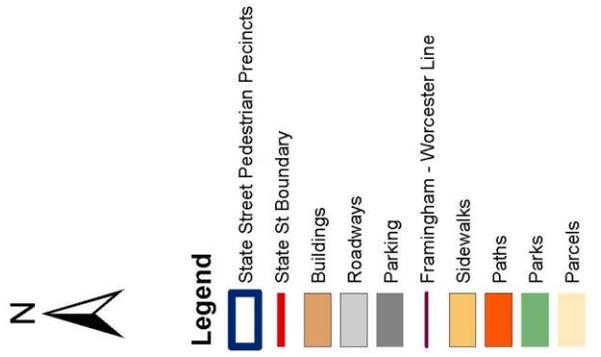
Private Lots**	General	Handicap	Total
Police Station	33	2	35
Funeral Home	23		23
City Sports	15		15
Haskins Auto	28		28
Behind 1-story shops near 462 Wash	12		12
Needham Bank	7		7
Whole Foods	181	8	189
Mobil	8		8
Zoots Carpet Cleaning	20	1	21
Private Totals	327	11	338

State Street Grand Totals	380
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State Street Area Base Map



1 in = 250 feet



Wellesley Hills

Wellesley Hills has many of the same qualities as Wellesley Square. There are a diverse set of businesses, adjacent residential neighborhoods, and a commuter rail connection. The Crosstown Trail runs parallel to Washington Street, providing a connection for pedestrians and bicyclists.



The Wellesley Hills MBTA commuter rail station is located at the heart of the Wellesley Hills district, which is a 40-minute ride from South Station on the Framingham/Worcester Line. The station is within walking distance of a large number of homes and businesses, meaning that residents commuting to work could walk from their homes.

Parking at Wellesley Hills station is provided, but not to the extent that it is provided at Wellesley Square (223 spaces at Tailby lot). There are no programs currently in place to provide preferential parking to Wellesley residents at the Wellesley Hills station. This type of program would encourage residents of Wellesley to use public transportation rather than rely on an automobile to get to work.

Land use in Wellesley Hills is primarily commercial, with some residential uses that include one- and two-family homes, apartment buildings, and mixed-use residential/commercial properties.

There is approximately 444,000 square feet of rentable commercial space in Wellesley Hills. About 45% of this space (201,000 square feet) is devoted to office buildings, and 18% (78,800 square feet) is devoted to retail uses. The remainder of uses includes a variety of commercial and civic space, such as an MRI Imaging center, the Wellesley Hills Post Office, and a mix of retailers from La Riviera (390 Washington Street) to the Route 9 overpass; such retailers include a few small food establishments, gyms, banks, and a gas station. Future development is anticipated including limited infill on Worcester Street and Washington Street.

Unlike Wellesley Square, there are few public parking lots. However, many businesses provide private parking lots adjacent to or behind their buildings. The variety of different land uses in the Wellesley Hills district makes shared parking possible. Parking

spaces vacated by office uses on evenings and weekends could be used by restaurants. A parking inventory is provided in **Table 4**.

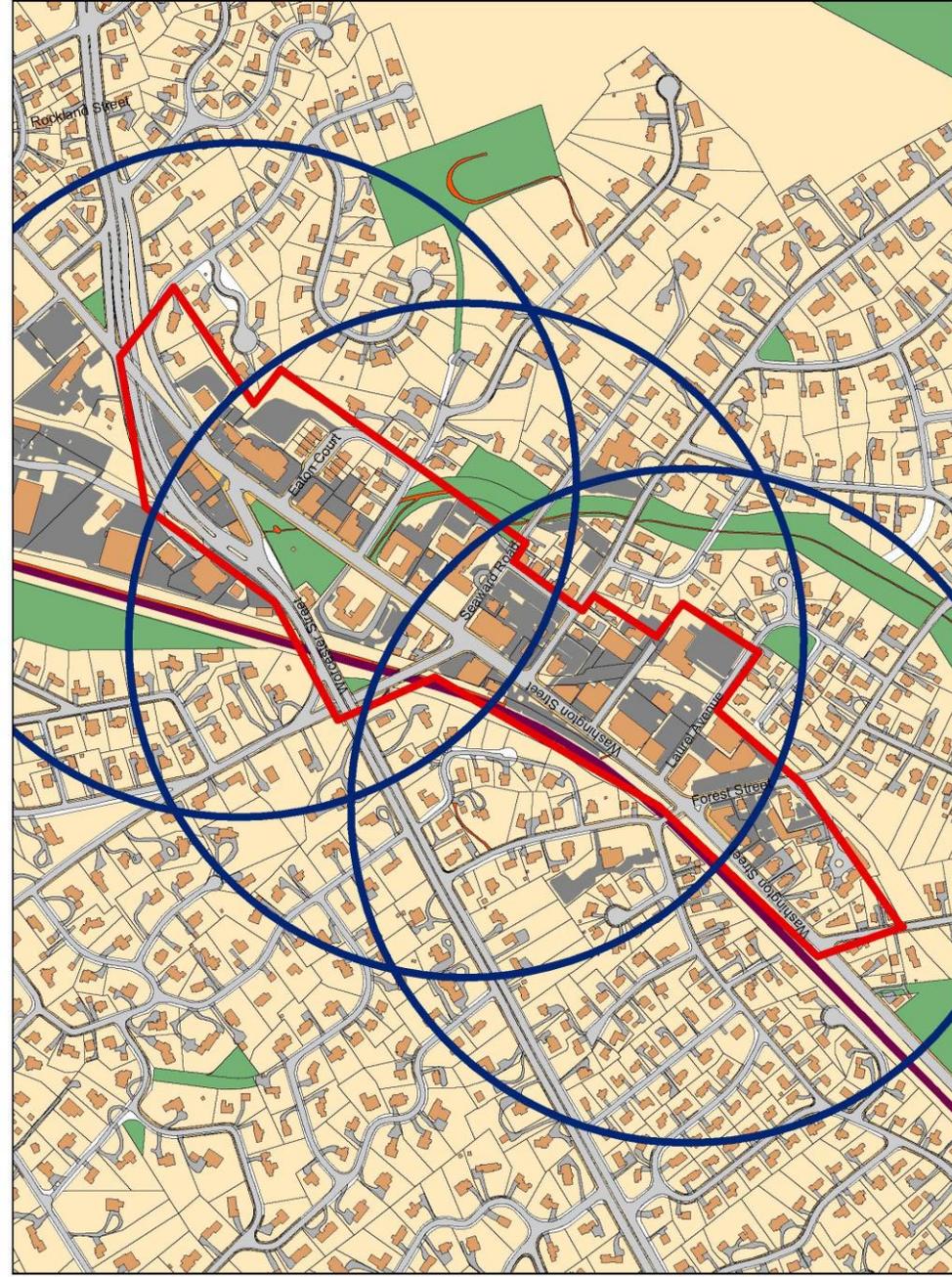
Table 4. Wellesley Hills Parking Inventory

Public Parking	2-Hour Spaces	4-Hour Spaces	10- Hour Spaces	Handicap Spaces	All-Day Spaces*	Totals
Wellesley Hills MBTA Station Lot				2	50	52
Eaton Court Lot	14			2	46	62
Lot Totals	14	0	0	4	96	114
Seaward Road	16					16
Maugus Avenue	10					10
Laurel Avenue	5					5
Prescott Street	7					7
Worcester Street "Extension"	12					12
Washington Street	128					128
Street Totals	178	0	0	0	0	178
Wellesley Square Public Totals	192	0	0	4	96	292

Private Lots*	General	Handicap	Total
Post Office lot (next to MBTA)	5		5
Corner of Washington/Forest	65	2	67
TD Bank/T Rowe Price Lot	63	4	67
Behind 380 Washington	16	2	18
Laurel Avenue lots	69	2	71
Behind 372 Washington	90		90
At 366 Washington	9		9
Behind 366 Washington	57	2	59
Behind 346-350 Washington	35		35
Behind 228-334 Washington	105	4	109
Behind 312-322 Washington	78	5	83
At Universal Unitarian Church	65	3	68
Between Maugus & Eaton	61		61
Walgreens lot+ adjacent	126	3	129
Behind Former Papa Ginos	23		23
422 Worcester Street	39	2	41
Larry Ng, Marathon Sports plaza	39	1	40
Private Totals	945	30	975
<u>Wellesley Hills Grand Totals</u>		<u>1267</u>	

*Based primarily on aerial imagery. Actual inventory may vary slightly.

Wellesley Hills Base Map



1 in = 400 feet



- Legend**
- Wellesley Hills Pedestrian Precincts
 - Wellesley Hills Boundary
 - Buildings
 - Roadways
 - Parking
 - Framingham - Worcester Line
 - Paths
 - Sidewalks
 - Parks
 - Parcels

Lower Falls

The Lower Falls district, which is adjacent to the Newton Town Line, has several office buildings and restaurants, as well as a new elderly housing complex, a medical office, and retail locations. The Lower Falls district is located adjacent to Interstate 95, and is 0.6 miles from the closest MBTA Commuter Rail station at Wellesley Farms. Nevertheless, high shares of visitors to the Lower Falls area arrive by automobile.



Public parking is provided along Washington Street, and in a public parking lot on River Street. Many businesses have private parking lots adjacent to or behind their buildings. A parking inventory is provided in **Table 5**.

One Washington Street, an office building with a large private parking lot empties out after work hours, leaving the parking lot empty. Parking is not permitted for outside uses at this lot, leaving a large number of parking spaces vacant outside of business hours. Shared parking arrangements could be made for this location at off-peak times.

A DCR path entrance is located within Lower Falls, indicating that there may be increased pedestrian and bicycle traffic in Lower Falls.

Land use in the Lower Falls district is primarily commercial. There are approximately 506,000 square feet of rentable commercial space in Lower Falls. Over 50% of this space

(263,000 square feet) is general office space. Approximately 30% (151,000 square feet) of the commercial space is devoted to the Waterstone at Wellesley elderly housing complex. The remaining 20% is devoted to uses such as retail, restaurant, and manufacturing offices. A Papa Razzi located at 16 Washington Street and a Dunkin Donuts located at 12 Washington Street seem to be the big draws area. In terms of future development, there may be potential for the Taylor Rental property at 26 Washington Street, but other opportunities in the district may be limited.

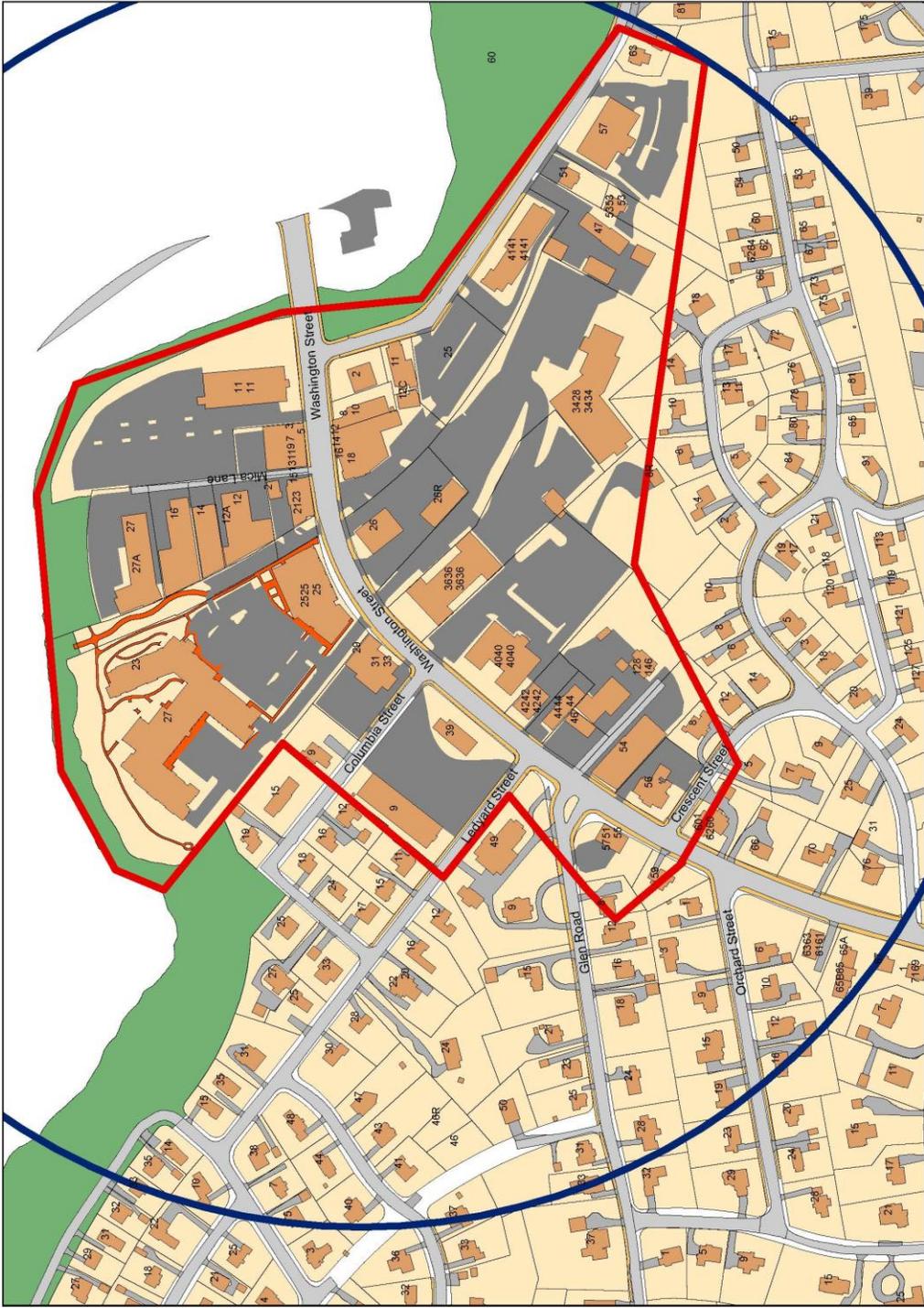
Public Parking	2-Hour Spaces	4-Hour Spaces	10-Hour Spaces	Handicap Spaces	All-Day Spaces*	Totals
River Street Lot	27	14	20	3		64
Lot Totals	27	14	20	3	0	64
Washington Street	18					18
Street Totals	18	0	0	0	0	18
Lower Falls Public Totals	45	14	20	3	0	82

Private Lots**	General	Handicap	Total
Behind Bank of America	32		32
Behind 39 Washington Street	83		83
23-27 Washington -- Surface	127		127
23-27 Washington -- Garage	139		139
Behind One Washington Street	198	4	202
Behind Brookline Bank building	73	2	75
Behind 36-40 Washington Street	419	11	430
Behind Express Gourmet/Papa Razzi	94	3	97
Adjacent to 419 River Street	12		12
Behind Taylor Rental	40		40
Private Totals	1217	17	1237

<u>Lower Falls Grand Totals</u>	<u>1319</u>
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*Based primarily on aerial imagery. Actual inventory may vary slightly.

Lower Falls Base Map



1 in = 200 feet



- Legend**
- Lower Falls Pedestrian Precincts
 - Lower Falls Boundary
 - Buildings
 - Roadways
 - Parking
 - Paths
 - Sidewalks
 - Parks
 - Parcels

2.3 EVALUATION OF EXISTING AND ALTERNATIVE PARKING REGULATIONS

The following is an evaluation of the current off-street parking bylaw, contained in Section XXI of the Wellesley Zoning Ordinance. The evaluation takes into consideration the existing conditions analysis of the five parking study areas, discussions with town staff and community stakeholders, and meetings with the Planning Board. The evaluation incorporates recommendations from town staff including the memo to the Planning Board in June 2013 regarding off-street parking regulations.¹ This evaluation also identifies where alternative or supplemental parking regulations and design standards are recommended based on case studies for similar communities and best practices. These recommendations are incorporated into the draft replacement parking bylaw and summarized in a parking standards alternatives matrix, which summarizes the most effective and appropriate combination of off-street parking requirements Town-wide and in particular zoning districts.

SECTION XXI. OFF-STREET PARKING.

General Comments

The current regulations contain the 1971 provisions which require a number of parking spaces based on the underlying zoning district and square footage of the building(s). This provision has been the dominant format that continues to present day. In general, these regulations demonstrate a “suburban” parking model where Wellesley is made up primarily of traditional and vibrant village centers and neighborhoods largely supported by rail transit, as well as MWRTA bus on a trial basis. This evaluation and resulting recommendations are intended to reflect the Town’s traditional development pattern and context, serving as the basis for changes to the standards by introducing new concepts, best practices, an improved and simplified table of parking requirements, and an administrative review process that is efficient and beneficial to the Town.

Several recommended alternative standards are proposed below that would provide for an adequate number of on-site parking spaces with several

¹ Memorandum from Meghan Jop to the Planning Board Regarding Overview of Existing Off Street Parking Regulations on June 12, 2013

opportunities to reduce on-site parking through best practices in shared and off-site parking methods. The proposed amendments are organized to apply town-wide with special provisions for selected districts to address particular conditions and characteristics. In many instances, the opportunity to share parking, secure off-site parking, utilize public parking, accommodate mixed uses with alternating peak parking demands, and other parking off-set techniques are recognized in the proposed amendments below.

Part A. PURPOSE.

The Purpose Statement is dated and focuses only on how automobiles serve those employed at or arrive at a business. Unlike many new parking bylaws, there is no mention of pedestrian access and safety, landscaping and aesthetic quality, or mitigating the negative effects of large commercial parking lots. There is also no mention of the value of shared or structured parking, joint access, transportation demand management, or accommodations for other modes of travel such as bikes or buses. The overall objective of a parking bylaw should be to provide adequate parking to support the various uses of a site while avoiding excessive parking lots that create dead, low-interest areas for pedestrians, and discourage people from walking to and within the district. These objectives are addressed in an alternative "Purpose and Intent Statement" in the new parking bylaw.

Part B. DEFINITIONS.

The current definitions are limited to those terms applicable to a traditional parking lot. Elsewhere in the bylaw, there are a variety of defined and undefined terms such as those used in the current parking table in Part D, Subpart 2 including "physical education", "physical recreation", "apartment house", "hotel", "inn" and "lodging house". Other terms in the bylaw such as "assisted elderly living" is defined in Section II – Definitions as "Assisted Elderly Housing". New and alternative terms are recommended in the new parking bylaw which reflect current best practices related to sustainable and low impact design (LID) such as "rain garden" or "pervious pavement" and other terms such related to bicycle facilities, parking structures, shared parking, staked or tandem parking, and transportation demand management. Additionally, it is recommended that new and revised terms be added to Section IA. DEFINITIONS of the Zoning Ordinance.

Part C. APPLICABILITY.

Generally, the grandfathering provisions under this section are unclear and may be limiting the Board (and the Town) from improving parking in redevelopment projects based on those that have come forward in recent years. This section also needs to be clear in terms of when change of use triggers certain parking standards including the number of spaces, trees and landscaping, lighting, and signage. For example, if a proponent were to alter landscaping in a nonconforming parking area (insufficient spaces or other reason) it is unclear if they would then need a special permit.

Specific recommendations for consideration in the new parking bylaw are as follows:

- There are parking requirements found in other sections of the Zoning Bylaws that should be cross referenced such as the Wellesley Square Density Special Permit and special parking provisions for various uses in Planned Development Districts (Section IXA). It should also be clear which provisions should prevail or possibly consolidated under this a new parking bylaw.
- The retrofitting of existing parking lots needs to be more specifically addressed.
- Under Part C.1., currently parking plans are only submitted for repaving, reconstruction or additions as permitted under section XVIA - Project Approval. Staff has indicated that the Town does not have good records of nonconforming lots and the number of parking spaces located in each, so determining the extent of parking enlargements or reconstruction is difficult.
- Part C.3. is the first of several grandfathering clauses, which states that for all buildings in Business and Industrial districts, only additions or new floor area in excess of what is on site as of March 31, 1982 require additional parking. It is unclear whether this provision would apply to structured parking in terms of ground floor coverage and FAR requirements.
- The last paragraph of Part C.3. is the second grandfathering clause that states all buildings in Business A, Industrial A or Education A as of April 4, 1983 are exempt from additional parking except for additions or new floor area in excess of what is on site. New floor area would require additional

parking. Again, it is unclear whether this provision would apply to structured parking in terms of ground floor coverage and FAR requirements. Alternative regulations should be clear that parking structures (decks, garages, tuck under, etc.) should not be counted in the building's square footage, or toward Floor Area Ratio under Projects of Significant Impact (PSI), which are triggered with development greater than 10,000 square feet and 150 parking spaces.

- Part C.4. does not account for mixed-use projects, nor does the table of parking requirements clearly allow for increased parking based on uses given the general parking calculations per district.
- Part C.5. is references SECTION XVII.C. Disaster Rebuild. However, it is unclear as to the thresholds for review and for what triggers additional parking requirements regarding reconstruction.

Part D. REGULATIONS AND RESTRICTIONS.

In general, Part D is unclear in terms of what actions trigger compliance with the parking requirements under this section. Additionally, the table of off-street parking requirements is confusing and dated in terms of the application of district and building footprint vs. use-based standards, terms and definitions, and the ratio of parking spaces to building use.

Subpart 1. GENERAL PROVISIONS

Subpart 1. General Provisions. Subsection a thru d, (pertaining to the Off-Street Parking or Storage Plan) is confusing and makes it difficult to determine the relationship between parking plan approval and site plan review. Given that many buildings have internal buildouts for new businesses, and the exterior and/or siting of most buildings is grandfathered – this provision is rarely used and property owners often alter parking lots without town approval (e.g. the Century Bank site).

Under Subpart 1. General Provisions. (pertaining to the parking for "two or more uses"), this section is rarely used since mixed-use projects are located in districts with single use parking provisions. For example, the language of the regulations for Business and Industrial Districts in Wellesley Square all have the same requirements of one space for each 150 square feet of ground coverage of building but not less than 3.2 spaces per 1,000 s.f. of floor area of building; therefore, the application to

the existing Bylaw is largely unnecessary and the provision could be eliminated.

Under Subpart 1. General Provisions. (pertaining to the parking for owner-restricted use of parking, staff has indicated that this provision has proven to be difficult because it allows property owners to create parking in village centers and then significantly restrict use. While this is a property owner's right, it may be worth exploring the possibility of designated portions of the lot for employee parking and supplemental public parking.

Subpart 2. REQUIRED PARKING.

The Off-Street Parking Requirements table is incomplete and unclear. As currently configured, there are too many irregularities with actual allowable uses in the zoning districts. Some specific issues and examples follow:

- All parking spaces required in the table are district-based then broken down by use. In areas like Wellesley Square, all uses are lumped together into one parking calculation.
- The calculation itself is difficult. The 1 space per 150 s.f. of ground area calculation only measures the footprint of the building. This equates to 6.67 parking spaces per 1,000 square feet of floor area. The second formula and calculation states "but not less than 3.2 spaces per 1,000 square feet of floor area". For a 2-story building, 6.67 is divided by 2 is 3.335 resulting in that all 2-story buildings only have to account for parking demands on the first floor. In effect, all 1-story buildings are penalized with a very high parking ratio of 6.67 parking spaces per 1,000 s.f.
- All parking calculations are only minimum requirements and there are no maximum parking limitations. This may not always be an issue but large surface lots can be unattractive, unsafe and use up valuable land unnecessarily. Additionally, there is no mechanism at present to impose an increase or decrease of parking on a case by case basis. There are special permit provisions to allow the count to be elsewhere, but not increased or reduced.
- "Apartment Houses" are permitted in the Business Districts; however, the first row provides requirements for Apartment Houses within Business District A but nowhere in the table are there requirements for Apartment

Houses within Business Districts.

- Parking areas for cars for sale or rent are currently exempt from these requirements. As a result, car dealerships are able to designate large areas for inventory, and such areas are currently not required to comply with key parking regulations such as landscaping, access and lighting.
- The asterisks at the bottom of the table are confusing and easily misinterpreted. For example, the “****” provision is only applicable to Lower Falls. There are also significant standards (such as the public parking factor) and they should be integrated into alternative standards for on-site parking reductions.
- New use-based parking requirements table should be consistent with the special parking regulations contained in the Planned Development Districts (cross references should be eliminated).
- The use requirements in the table are somewhat higher than found in newer bylaws. The demand for parking (and associated requirements) is highly dependent on the context within which a given use or structure is located. Factors such as proximity to other related uses, availability of public transportation, the density and mix of uses, and the ability to share parking with nearby uses are all factors which influence the demand for individual and dedicated off-site parking. These factors need to be considered in the alternative parking standards.

As an alternative, the Off-Street Parking Requirements table should be based solely on use rather than the current district and building-based standards. Additionally, mixed use districts such as village or neighborhood centers have significantly more opportunities to share parking than single use districts and the alternative parking standards should reflect this.

Where practical, shared parking should be strongly encouraged in the alternative standards and factored as an offset into the required number of spaces. For example, alternative standards should allow and encourage a reduction if multiple or mixed uses are compatible and can demonstrate that such a reduction would still provide adequate parking. The proposed amendment would make it possible for the Planning Board to grant a special permit for the reduction in required spaces upon a reliable showing of lesser parking need for a particular mix of use.

Subpart 3. DEVELOPMENT STANDARDS.

DESIGN.

- Under subsection a. (pertaining to parking spaces and aisle dimensions), some Planning Board members are concerned that the 8'6" minimum parking space width may be insufficient in certain applications. Whole Foods has been identified as an example where this width appears to be of concern. While a width of 8'6" is consistent with best practices, alternative standards may need to recognize certain applications such as supermarkets, garden nurseries, or building supply stores where customers are maneuvering in and around their vehicles with shopping carts.
- Under subsection a. (pertaining to compact parking spaces and aisle dimensions), staff has indicated a concern that the width requirements for compact spaces are very narrow at 7'6". Additionally, the thirty percent (30%) allowance for compact car spaces appears to be too high. Alternative standards should recognize the need and benefit are encouraging compact spaces but possibly at a smaller percentage and 8' minimum width requirement.
- Under subsection d. (pertaining to access by driveway), there may be opportunities to allow for cross access and internal circulation between sites which is lacking in existing regulations but should be addressed in the alternative standards to facilitate shared parking and access.
- New design provisions should be included in the alternative parking standards for car sharing services (i.e. Zip Car), electric car charging spaces, stacked parking, tandem parking, and structured parking.

CONSTRUCTION.

- The construction standards currently have no provisions or incentives for sustainable and low impact design applications. Alternative standards should integrate these best practices for stormwater volume and treatment such as the application of pervious pavers, shade trees/ground cover, bioswales/rain gardens, underground storage, green roofs, stormwater planters, shared or district stormwater facilities, and infiltration parks.

LANDSCAPING.

- The landscape provisions are not specific about the type, placement, or installation of trees and other plantings. Alternative standards should identify appropriate shade and understory trees that are non-invasive, drought resistant and salt tolerant, climate appropriate, and make attractive street trees.

SCREENING.

- In subsection b and c, staff has indicated that landscape-screening standards, including the 5' wide by 5' tall vegetative buffer requirement, have not been enforced and might be impractical and undesirable. Alternative standards should clarify appropriate placement of screening materials. For example, screening a parking lot from a public/private way may impede visibility of storefronts depending on the configuration of the building.
- In subsection d, maintenance should address trash and snow storage.
- In subsection e, lighting standards should be a separate section under Development Standards. Alternative lighting standards should expand on the requirements with specific, objective provisions. For example, on and off-site footcandle levels should be established. Additionally, alternative lighting provisions should include full cut off shields, solar lighting, or alternative energy options.

Part E. ADMINISTRATION.

The Administrative section should be moved to the front of the bylaw after Purpose and Intent. The current provisions do not have a procedure to allow for minor modifications or renovations to pre-existing nonconforming parking lots. Staff indicates that many business owners do not try and improve parking areas for fear of triggering substantial review. This procedure should be addressed in the new parking standards.

Subpart 1. PERMITS.

Subpart 2. SPECIAL PERMITS.

The current parking standards allow for redistribution of required parking on private lots or public parking areas, but do not allow for a straight reduction of parking spaces. Alternative parking standards should address these opportunities as well as possibility of a payment-in-lieu of provision where an off-street parking fund would be created which could be used to fund structured public parking, wayfinding and other enhancements in appropriate districts.

OUTLINE OF POSSIBLE ALTERNATIVE/NEW PARKING STANDARDS

General Provisions

The alternative or new standards will address the following general parking provisions:

- Purpose and intent
 - Maximize use with least number of spaces necessary
- Administrative procedures
 - Review of new development and expansion, reuse, renovation, and reconstruction of existing buildings and uses
- Required On-Site Parking Spaces
 - Minimum and Maximize standards based on use
- Definitions and applicability
 - Buildings and use
 - Districts (downtowns to neighborhoods)
- Required off-street parking spaces
 - Parking Table by individual and mixed uses
- Parking limitations and exemptions
 - Minimum/maximum: surface and structured
 - Exemptions (i.e. outdoor dining, vendors, etc.)
 - Reserved parking areas (potential future activation)

On-Site Parking Reduction Toolbox

The only provision in the current bylaw for on-site parking reduction is if a parcel is

within 600 feet of public parking lot with 50+ spaces, off-street parking is required at 2.5 spaces/1,000 GSF (1/400) excluding uninhabitable basements. Through alternative and new parking standards, the following on-site parking reductions methods will be considered:

Table 6. Potential On-Site Parking Reduction Methods

Maximum Parking Limitations	Allowing no more than 100% of required off-street parking spaces except by special permit with unique circumstances
On-Street Parking Credit	Allowing 1 off-street space reduction for every on-street public parking space in front of business.
Off-Site Public Parking Credit	Reduction by special permit up to determined % if public parking lot with 20+ spaces is within 300 feet and demonstrated availability.
Off-Site Employee Parking Credit	Reduction by special permit up to determined 0% if public or private parking lot is within 700 feet and demonstrated availability.
Payment to Public Parking Fund	In lieu of off-street parking and by special permit, town may accept payment made per each space as determined by BOS to be used in construction of new public parking spaces.
Proximity to Bus Transit	By special permit, town may reduce off-street parking by up to 5% for commercial and 10% for residential if main entrance is within 400 feet of bus stop and shelter.
Proximity to Rail Transit Station	By special permit, town may reduce off-street parking by up to 10% for commercial and 20% for residential if main entrance is within 2,640 feet of station.
Car-Sharing Program	By special permit, town may reduce off-street parking by 5 spaces per car-share vehicle where active program available to residents or employees within 700 feet.
Carpooling/Vanpooling	By special permit, town may reduce off-street parking by 5% if use greater than 50,000 s.f. institutes and maintains active program
Shuttle Service	By special permit, town may reduce off-street parking by 15% if uses which institute and maintain active personal shuttle program
Transportation Mitigation Plans (TMP)	By special permit, town may approve TMPs for uses requiring large scale parking lots of 250+ through a combination of reduction methods.
Curb Cut Reduction and Internal Connectivity	By special permit, the town may reduce off-street parking if pedestrian safety improvements are through reduced number and width of curb cuts.
Public Parking Reserve	In lieu of off-street parking, town may accept a permanent easement for future public parking lot subject to approval by BOS.
Private Parking Reserve	In lieu of off-street parking, town may accept a preserve on site equivalent to area that parking would have been required. The reserve area must be equal to 300 s.f. per parking space reduced. After 1 year the town can determine whether reserve area is needed for additional parking or waived allowed for other uses.

Additional on-site parking reductions will also be considered such as for mixed uses on a site or within a building, shared parking agreements on adjoining lots, valet parking program and tandem parking, and incentives for pedestrian and bike access, connectivity and safety improvements. The general criteria for reducing on-site parking will consider:

- Differences in peak hour demand
- Site occupants less reliant on personal vehicles
- Linked trips common in district (i.e. village centers)

- Access to alternative transportation modes and methods
- Availability of remote parking (public and private)

The criteria for shared parking agreements will consider:

- Coordinate peak demands - hour and day
- Business to business cooperation
- Business and institution (public, church, etc.)
- Valet parking - strategic placement and centrally managed

Parking Placement and Design

- Access management and curb-cut design
- Internal circulation and external connectivity
- Placement of parking
 - Relationship to buildings and street
- Intermodal design
 - Bus facilities, internal pedestrian connections, bicycle parking
- Landscaping
 - Shade, control circulation, provide access, screening where needed
- ADA spaces, compact autos, alternative fuel vehicles, stacked/tandem parking, drive-thru lanes/windows
- Loading, storage and screening
- Incentives for PARKing and mobile vendors

3.0 PARKING CASE STUDIES FROM OTHER MUNICIPALITIES

HSH conducted a review of several municipal parking policies and regulations for commercial and mixed-use areas from around New England. Seven case studies were identified that we felt had important common characteristics with Wellesley in terms of historic development patterns including traditional villages, neighborhoods, commercial corridors and the presence of large educational institutions. These municipal case studies include: Burlington VT, Newton MA, Amherst MA, Northampton MA, Concord MA, Durham NH, and Arlington, MA. Collectively, they provide a variety of parking standards that bear consideration in Wellesley.

The following summary provides a cross-section of information on the key parking standards in each of these communities:

Burlington, VT

The stated purpose of Article 8 – Parking is to provide adequate parking and loading, proper circulation, reduce hazards to pedestrians, reduce congestions, encourage alternative modes of travel, and reduce dependence on the single-occupancy vehicle. The regulations apply to all new or altered buildings and change of use; waivers can be requested, and buildings and uses in certain areas such as downtown are exempt.

The bylaw establishes parking districts, and recognizes that the demand for parking is highly dependent on the context within which a given use or structure is located. Factors such as proximity to other related uses, availability of public transportation, the density of land uses, and the ability to share parking with nearby uses are all factors which influence the demand for individual and dedicated off-site parking. There are three (3) parking districts as follows:

- Neighborhood Parking District - Establishes the baseline of parking requirements throughout the city where the demand for offsite parking is largely dependent on the needs and characteristics of an individual site or land use.
- Shared Use Parking District - Reduces the requirements from the baseline standards; recognizes that opportunities exist to share parking demand between related nearby land uses, and that travel to and between these uses may not be strictly automobile dependent.
- Downtown Parking District - Further reduces the requirements from the baseline standards. Recognizes that extensive sharing of parking demand between nearby land uses occurs; that a majority of travel to and between land uses is independent from an automobile; and that an array of public parking facilities

and frequent transit service greatly reduces the need for independent parking for individual land uses.

The bylaw includes an extensive list of uses on the Minimum Off-Street Parking Requirements Table under the categories of residential uses, special residential uses, and non-residential uses. The minimum requirements are also organized by the Parking Districts, including Neighborhood, Shared Use, and Downtown, with the least number of spaces required in downtown. In general, the required spaces by use are less than many other conventional parking bylaws.

The bylaw allows for a maximum number of spaces equal to 125% of the required amount on the use table. Waivers from the minimum requirements may be granted if an applicant submits a Parking Management Plan that specifies why the parking requirements are not applicable. Waivers of up to 50% are allowed for the adaptive reuse of a historic building in any mixed use district. The waivers are based on specific review criteria requiring the applicant to demonstrate the current parking need; that more parking will continue to support pedestrian activity and transit use; and that the parking area can be redeveloped in the future if it is not needed.

The parking reduction and exemption methods include structured parking, public parking availability, carpool/vanpool, car-sharing parking, and alternative fueled vehicle parking. Shared use is also allowed, and overall parking totals can be reduced if demonstrated that peak hour demands are off-set by the combination of uses.

Off-street loading space is required for all non-residential uses as determined through site plan review. It must provide safe access and circulation, be screened from view, and minimize conflicts with pedestrians.

Minimum parking dimensions provide for parallel, 45°, 60°, and 90° parking. Angled parking spaces must be 9'x20', parallel spaces must be 9'x22', and aisle widths are 10 feet for one-way or 20 feet for two-way flow. Angled compact cars spaces must be 8'x18', and parallel spaces must be 8'x20'. Up to 15% of a parking lot can be designated for compact cars. There are specific standards for handicapped parking requiring compliance with current ADA standards, including a 5' adjacent access aisle.

Tandem parking is allowed for single family detached homes, duplexes, and accessory units. Stacked (or valet) parking is allowed for other uses with attendant and agreement with the city. The bylaw has extensive bicycle parking standards based on use, location, and design. The bylaw also has institutional parking regulations requiring post-secondary and medical institutions to parking plans.

Off-site satellite parking, is allowed as follows: 50% with 600 feet in Neighborhoods Districts; and up to 100% within 1,000 feet in Downtown and Shared Use Districts, measured by walking distance along a sidewalk or other pedestrian path.

Parking placement standards require parking to be located behind the building frontage in order to maintain pedestrian-oriented uses in the downtown area. Parking is also restricted from the front yard in all residential districts where abutting public streets except alleys. Residential driveways can be no more than 18 feet wide.

Northampton, MA

Section 350-8 Off-Street Parking and Loading Regulations indicated that the required number of parking spaces is applicable to every new structure; enlargement of existing structures; and new or expanded uses. Exceptions include in Entryways; General Business Districts where no building expansion occurs (change of use is exempt); and in Central Business Districts (downtown) where a second story is added to existing one story, which is more uniform and consistent with historic development characteristics.

The table of required parking is use-based, which consolidates several uses into broad categories. The required number of spaces is broken down by Central Business and districts with significantly less or no parking required downtown. Generally, the required spaces are less than other municipal regulations. Some or all of the required parking spaces may be waived in the Central Business district by payment to the City's Downtown Parking Reserve Account of \$2,000 per required space. The fund is used exclusively for land acquisition, and design and construction of additional downtown parking spaces which are not for maintenance.

Off-street loading provisions must be adequate for all uses except agriculture as determined through site plan review. A minimum of 1 space each is required for retail, wholesale, and industrial over 25,000 square feet.

Shared parking is allowed and may reduce parking requirements by up to 20%, except in the CD district where parking is not required. Special permit reduction can be greater than 20% based on substantially different peak hour demands between uses. Bike parking (i.e. racks) is required for all uses that have at least 10 required car parking spaces.

Remote/Satellite parking is allowed by special permit if within 500 feet, or within 1,000 feet if: shared between adjacent uses; employees and patrons are likely to use; and

pedestrian traffic patterns justify longer distances. College or educational parking must be on same or contiguous property.

Parking space and aisle dimensions include standards for back up maneuvering, and a 5-15 foot front setback in residential districts. Spaces must be a minimum of 8.5 x 18 feet. There is a maximum access of one driveway curb cut per lot without a special permit demonstrating need based on safety issues. Maximum driveway width is 15 feet for residential and 24 feet for other uses, but the Planning Board may approve up to 30 feet through a site plan review based on safety concerns. Shared driveways are permitted, as well as tandem and stacked parking.

Includes landscaping and screening requirements for surface lots greater than 5 spaces, requiring at least 1 shade tree of 2" caliper for every 15 spaces. Suitable plantings and fencing is required to screen adjacent residential uses. For parking lots over 75 spaces, planting strips are required which must also contain a pedestrian sidewalk of 6 feet minimum. Pervious pavers are allowed.

Durham, NH

Section 175-110 Off-Street Parking and Loading indicates that the required number of parking spaces is applicable to all erected or enlarged structures, and new or added uses.

Parking space and aisle standards require 9x18 foot spaces with 22-foot aisles for 90° parking, and 9x17 foot spaces with 20-foot aisles for 60° parking. The width can be reduced if combined with an 8 foot access aisle for handicapped spaces. Compact spaces should be 8x16 feet and can be designated for 30% of required spaces. Driveways in the front yard may not be wider than 22 feet. Bike racks are required for parking lots with 10 or more spaces.

One driveway is permitted for residential districts within the first 200 feet of frontage, and a second within the next 100 feet. Two driveways are permitted in commercial districts. Shared driveways and internal access are allowed and encouraged. These are an incentive by increased lot coverage.

The table of required parking spaces is used-based and lists uses categorized by residential and commercial uses. Generally, the required spaces per use are fairly high. Required parking can be waived in the Central Business District with a one-time parking impact fee for the additional number of spaces required minus those existing on site.

Parking placement standards encourage that buildings be located close to the street, with parking behind to improve pedestrian safety and vitality. Standards allow parking in the front yard for single-family homes and duplexes. Parking in the side and rear yard is allowed for all uses with a 5-foot landscaping strip, and solid screening from adjacent uses and public right-of-way. Parking for a commercial residential mixed use cannot be located in setbacks adjoining single-family homes or duplexes. Shared parking is allowed for off-site employee parking, and is approved by the Planning Board. Businesses outside the CBD are required to have adequate off-street loading spaces or bays. The number of required loading spaces is based on the size and type of use.

Parking lots greater than 5 spaces are subject to landscaping and screening requirements. At least 5% of the lot, in addition to the required 10-foot buffer strip along the public right-of-way, must be landscaped. Lots must be broken up into sections containing no more than 40 spaces, with integrated landscaping and bio-retention features. Perimeter landscaping along a street must be native species. Islands, peninsulas, and medians must be a minimum of 6 feet wide and should be evenly distributed; low impact development (LID) applications are encouraged. Some islands must include pedestrian walkways. Tree species must be selected from a recommended list. A performance bond is required for landscape installation and maintenance for one year. Lighting must be directed away from adjoin properties and fixtures must be cut-off. Dust free surfacing is required and infiltration of stormwater on site is strongly encouraged.

Amherst, MA

Parking & Access Regulations under Article 7 of the Zoning Ordinance are applicable to all conversions or increase in dwelling units, dimensions of buildings, or uses excluding the Education District. The minimum required parking spaces are use-based and loosely grouped by residential, accommodations, places of assembly, and commercial uses. The minimum standards are fairly high by use. Some requirements are based on zoning district with higher amounts for the first 10,000 GFA and ground and upper floor space, which is similar to Wellesley but more clearly organized and defined. Handicapped parking is required and based on a percentage of the total number of spaces in the lot, at a ratio of 1 handicapped space per every 10-20 total spaces. Bike racks are also required for lots with 10 or more parking spaces.

Shared parking is allowed for lots with 100 to 120% of required spaces by special permit based on specific criteria. Lots with more than 120% of required space may share parking by right. The Municipal Parking District is an overlay district covering the downtown area. The purpose is to facilitate dense mixed use in Amherst Town Center

by eliminating on-site parking requirements. Only dormitories and hotels are required to have on-site parking.

Design and landscaping standards are applicable to new or altered parking spaces of 5 or more spaces. A detailed and scaled Parking Plan is required and certain standards may be waived by the building commissioner. Lighting must be downcast and directed, eliminated during off hours, and cut-off fixtures are required.

Parking space dimensions must be a minimum of 9x18 feet and by special permit small car parking space of 8x16 feet may be permitted. Parking must be setback from the building by a minimum of 8 feet. Driveways must be at least 10 feet wide for one-way and 18 feet to two-way use. The maximum driveway width is 24 feet and minimum curb radius is 15 feet. Driveway must be setback at least 75 feet from an intersection. Common driveways are permitted. Waivers to these standards may be granted by the SPGA. Adequate loading areas are required for all commercial and industrial uses.

Parking lots with 10 or more spaces must include 10% of the area in landscaped open space. Lots with 25 spaces or more must have 4-foot landscaped islands throughout area which define end caps and rows between aisles and control circulation. Lots with 5 spaces or more are required to screen from adjacent properties and street to a minimum height of 3 feet using grade depressions, hedges, walls, or other natural or artificial division. Pervious or porous paving must be integrated into new or renovated lots to the extent feasible.

All provisions in the parking bylaw may be waived by the SPGA for compelling reasons of safety, aesthetics, or site design. Waivers are also based on detailed criteria involving peak hour off-sets and submission of a parking management plan.

Newton, MA

The stated purpose and intent of Article 3, Section 30-19 is to design and operate parking on site so that it reduces hazards to pedestrians on the sidewalk, nuisances to adjacent properties, congestion, and glare from headlight. It's also intended to protect the visual quality of the city.

The parking standards are applicable to all erected or enlarged structures, and new or added uses. All pre-existing buildings are uses are exempt except for the portion of the lot where additional spaces are required by expansion or change in use. The number of new spaces required is based on a formula where the total number of spaces required under the bylaw at the time the building or use was permitted plus the number of

spaces presently on site is subtracted by the total spaces required under the current bylaw. Municipal parking lots are not allowed to meet the number of spaces required in the bylaw.

The minimum required parking spaces are use-based and fairly high for residential, office and retail uses. Some standards are based on the size of the building and number of employees as well as the use. Parking lots of 5 spaces or more require an off-street parking and loading plan drawn to scale and containing detailed information on design, layout and materials used. Bicycle parking is required for parking spaces requirements are 20 or more spaces in the amount of 1 bike space per 10 car spaces to a maximum of 30 bike spaces. Bike spaces must be at least 2' wide by 7' long.

Satellite parking is allowed by special permit if within 500 feet and where there is not enough space on site to meet requirements. In parking lots of less than 5 spaces, no parking is allowed in required setbacks except one space in the front yard in residential districts. In parking lots of 5 or more spaces must meet setback requirements and a minimum of 5 feet from any street or building.

Parking space and aisle dimensional requirements include 9x19 foot stalls of all angled parking and 9x21 feet for all parallel parking. Aisle width ranges from 12 feet for parallel parking to 24 feet for 90° parking. Two way stalls must be a minimum of 20 feet. Driveways must be a minimum of 12 feet and maximum of 25 feet. Handicapped spaces are required based on the overall size of the lot (i.e. 1 space for lots 6-25 spaces). Handicapped stalls must be at least 12x19' for angle parking and 24 feet long for parallel parking.

Landscaping and screening is required for parking lots greater than 5 stalls. A 5-foot densely planted perimeter screening is required for abutting streets and properties. Minimum height is 3 ½ feet and must form as year-round screen. As an alternative, a wall or fence can be used, or a landscaped earth berm at least 3 feet high and 18 feet wide. Interior landscaping of 5% of the total area is required to lots with 20 spaces or more. Planting area must be at least 20 square feet and a 3" caliper tree is required for every 10 spaces. The tree species must be rapid growth, hardy and suitable for the location. Lighting for security must be at least 1 foot-candle and directed to reduce glare onto the street.

Off-street loading is required for commercial, recreational, and institutional based on the square footage of the building or use.

Arlington, MA

Section 8.0 covers off-street parking and loading. In terms of placement, residential parking in the front yard is only allowed by exceptional circumstances which are a positive aesthetic goal. However, the general statement also required for every new structure, land use, and expansion/change in existing buildings and structures. The need for parking should be determined on a case by case basis and not necessary required for all new or expanded uses.

The Table of Off-Street Parking Regulations in Section 8.01 indicates the number of off-street spaces required for various uses. The ratios are reasonable for residential dwelling types recognizing less need for parking spaces in apartments based on the number of bedrooms as well as assisted and senior living units. The ratio of spaces for health clubs and retail is slightly high (1/300 s.f.) and mixed uses require and sum of various uses computed separately (not allowing to shared parking in off-set and portion of the required overall spaces).

Loading and unloading requirements in Section 8.02 indicate that all commercial buildings must have an off-street loading area but some may not need a separate facility and may be able to receive deliveries from the street. According to this section, the SPGA may grant a special permit for the size of the loading area.

Section 8.05 provides for shared parking on combined facilities and the same or adjacent lots by special permit or where projects are subject to Environmental Design Review (EDR). Under the same review process, remote or satellite parking may be permitted but must be within 600 feet with a long-term agreement from the property owner. This provides some needed flexibility in requiring all parking spaces on the same lot. However, it does not relieve the overall number of spaces required whether for individual or multiple shared uses.

Under Section 8.07 parking access and placement standards are provided for Residential and Business District. For 1 to 3-family dwellings, parking is not permitted in the front yard setback but is allowed in the side and rear yard. Only one driveway in the less intense residential districts is permitted (2 by special permit) and cannot be wider than 20 feet. In the Business Districts (B1-B5) no parking is permitted in the front yard setback and driveways in front of the building are not allowed without a finding of public necessity and interest by the ZBA or ARB through the EDR process. This provision appears to protect the pedestrian environment but it's still possible to locate the parking in front of the building if it is pushed back far enough from the street.

Section 8.10 requires parking lots of 5 spaces or more to be surfaced with permanent materials and provides for sustainable design including porous asphalt. Section 8.11 allows the ZBA or ARB under EDR to reduce the number of required parking spaces where public parking lots are located within 1,000 feet of the building. However, no provisions are included regarding the size, capacity, or availability of these public parking facilities.

Section 8.12 includes parking design standards which appear to be reasonable and reflect typical design standards. Under the same section, the ZBA and ARB may allow a parking reduction of up to 80% of the required number of parking spaces in the Table of Off-Street Regulations under conditions unique and reasonably justified. Up to 20% of surface or structure off-street parking may be sized for compact cars and designed accordingly.

Landscaping and screening and setback requirements are made where parking lots abut residential uses. Landscaping of 8% of the total interior area is required for parking lots greater than 25 spaces. (does not include required perimeter landscaping and buffering areas). Planting strips must be at least 4 feet.

Section 8.13 requires bicycle parking facilities in parking lots of 8 spaces or more. However, there are no additional “end-of-trip” facilities for larger employers to encourage and accommodate more bicycle commuting. Additionally, places of worship are exempt from this requirement which would be a likely use for bicycle access.

Overall, the parking regulations appear to be reasonable by allowing significant reductions in the number of required off-street spaces by factoring in public parking proximity, satellite parking and further reductions under development review. This is a particularly important consideration for business development in the village centers. Other provisions that the town is considering include: further off-sets for tandem parking, on-street public spaces in front of stores, proximity to bus transit stops and shelters, and valet parking; structured parking design standards; internal access and circulation standards; and shade tree size and placement standards for interior and perimeter areas.

Concord, MA

Section 7.7 Off-Street Parking, Loading, and Design Regulations includes a stated purpose of providing sufficient off-street parking, reducing hazards to pedestrians, reducing congestion and nuisances to adjacent properties, and reducing accelerated stormwater and unattractive views of large expanses of pavement.

The table of required parking is use-based, identifying several uses in broad categories including residential, institutional, business, and industrial. Generally, the required spaces are similar to many other conventional municipal standards. The required number of spaces is applicable to all new buildings and uses as well as existing ones that “increase parking demand” by increase in gross square footage or use, and the increased parking spaces must meet the table standards and apply to the entire building. Section 7.7.7.2 prevents owners of required parking spaces to use them for commercial or public purposes, meaning that they cannot charge tenants or customers for the use of the lot.

Joint parking is allowed in any zoning districts and between different uses based on the difference in peak hour use as determined by the Planning Board. A written agreement for joint parking must be filed with the town. Mixed use facilities are also permitted but each use’s parking demand is calculated separately.

Parking lots are required on the same lot as the primary building but the Planning Board may allow remote parking when it is within 600 feet, with same ownership or long-term lease, if the lot is in the same zoning district, and where the lot will not generate more traffic. The Planning Board can also require reserve parking for as much as 50% to determine if all required is needed at a future time. Off-street loading provisions must be adequate for all uses and are based on gross square feet of individual uses.

Parking space and aisle dimensions include minimum standards for parking stalls of 9x18 (8x16’ for small cars) and 24 foot two-way aisles. There are additional requirements for small cars and handicapped spaces.

There are landscaping and screening requirements for surface lots greater than 5 to spaces based on site plan review standards. Lighting standards are intended to minimize glare, light spillover, and light pollution.

4.0 RECOMMENDATIONS

The amount, distribution, and availability of public and private parking (and the perception of adequate parking) is critical to the vitality of Wellesley's village centers, commercial corridors and neighborhood districts. In the traditional village areas, the historic building pattern should be preserved with as much public on-street and off-street parking as possible for general use by local businesses, services, civic/institutional uses, and residents. On-going parking issues for Wellesley include the strategic expansion of public parking, connection of public and private parking areas, management of public and private spaces to facilitate a higher turnover of spaces primarily intended for customer use, and designation of longer-term remote spaces for local employees and commuters. However, the highest priority and top recommendation in this report is to adopt a new parking bylaw which provides clarity, flexibility, efficient land use, context-based parking placement, sustainable design standards, and encourages access to development by pedestrian, bicycle, transit facilities as well as vehicles.

The following recommendations are intended to guide Wellesley in making improvements to policies, programs, regulations, and infrastructure pertaining to public and private parking in the five study areas and throughout the community.

Adopt Alternative and New Parking Regulations.

Specific recommendations for alternative and new off-street parking regulations are identified in Section 2.3 of this report and several key elements are further described below:

Adopt Parking Placement and Site Design Standards.

Parking placement and access are key contributors to the pedestrian environment and walkability. A higher number of curb cuts over the sidewalk and the separation of the sidewalk and buildings by parking lots in the front yard significantly diminish the walking experience and visibility of storefronts by pedestrians, bicyclists and drivers. For example parking lots along the Linden Street and Washington Street corridors are predominately in front of the buildings with several locations having multiple and wide curb cuts. Recent development projects such as Linden Square and Whole Foods provide excellent examples of the alternative, where infill development is placed along the sidewalk and parking is located along the side and rear. However, this development scenario is not the norm and there are many other areas where rear/side lot placement,

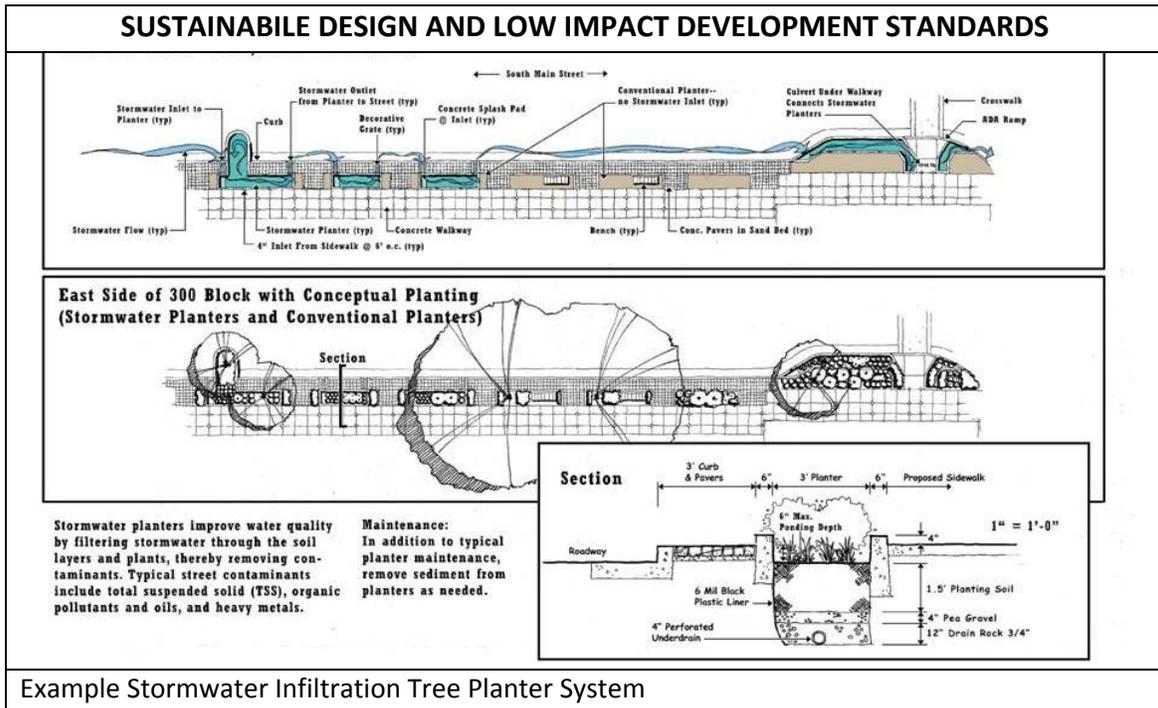
shared access, and internal connectivity between parking areas can be improved. Additionally, there are opportunities to improve access between the public sidewalks and the parking area such as by pedestrian alleys and internal sidewalks. The alternative parking standards address these placement and site design issues.

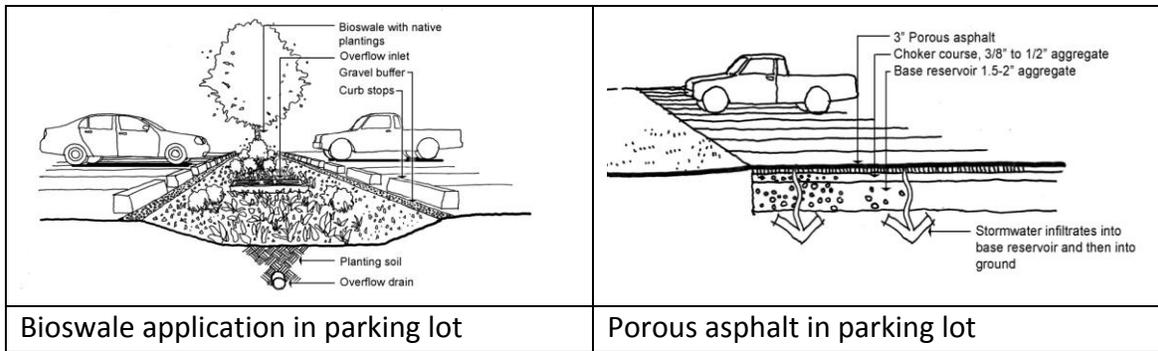
GENERAL SITE ACCESS, PARKING AND CONNECTIVITY STANDARDS	
<p>The diagram illustrates a site plan for a proposed building. A 'MAIN ROAD' is on the left. A 'PROPERTY LINE' is shown as a dashed line. A 'BUILDING SETBACK LINE' is also shown as a dashed line. The 'PROPOSED BUILDING' is a large grey rectangle. Callouts A through J point to specific features: A is the primary building entry; B is a screened parking area; C is the front facade; D is landscaping along the setback line; E is an adjacent parking lot; F is a sidewalk; G is a loading dock; H is a tree; I is a sidewalk on the main road; J is a service area.</p>	
<p>General Design Standards - The building should be brought forward as close to the street as possible, with parking and other services tucked around the side and back. This helps to define the identity of the street and gives the business maximum public exposure.</p>	
<p>The parking lot areas should be designed in regular, rectangular shapes. Irregular wedge shaped parking areas created by following angled property lines should be discouraged. Only create as much paved area as absolutely necessary.</p>	
A.	<p>The primary building entry should always be placed at the front of the building (facing the front lot line) and be clearly identifiable from the street. In this example, an entry vestibule is created which faces the front, side and rear parking area simultaneously.</p>
B.	<p>Parking should be placed at the side or rear of the lot and screened from view whenever possible. Where possible, break the parking areas up into groups of no more than 20 spaces per area (35 for large development projects) separated by landscaping areas.</p>
C.	<p>Place as much of the building width at the front of the lot as possible to maximize front façade exposure to the traveling public. The front façade should be kept parallel to the street.</p>
D.	<p>To reinforce the “street-edge”, align with adjacent buildings which are also close to the front setback line. Landscaping can also be used to reinforce this line.</p>
E.	<p>Whenever possible, attempt to link with adjacent parking lots or provide shared parking areas which can serve neighboring buildings simultaneously. This provides a secondary means of access to the site and can ease congestion on the main road.</p>
F.	<p>Provide sidewalks for the full width of the property with a direct link to the primary building entry.</p>
G.	<p>Loading docks, service areas and trash facilities should be located at the rear of the building and not visible from the street. Fences, walls or landscaping can be used to shield them from view.</p>
H.	<p>Incorporate any existing, older trees into new site plan development whenever possible to reduce waste and utilize good shade trees.</p>

I.	Minimize the amount of curb-cuts by having a single driveway in and out of the property from the main road whenever possible. Secondary access points from side roads are encouraged. Curb cuts should only be as wide as necessary to accommodate needed lanes. Curb radiuses should be kept to a minimum.
J.	Landscaped islands and other green space should be consolidated into useful areas, and not just narrow strips of grass or plantings.

Adopt Standards for Sustainable Streetscape and Parking Areas.

Install Green Streetscapes - Green streetscape capture, temporarily store, and treat road runoff at its source by incorporating planting strips and vegetated water catchment and filtration devices in the form of small rain gardens, bioretention systems, and flow-through planters. These low impact development (LID) applications allow stormwater from the street to enter planting strips and tree pits through cuts in the curb where the plant material removes impurities and allows water to naturally infiltrate or be stored elsewhere. Water-loving plants and those that are able to remove the impurities while thriving so close to traffic and in more urban environments are used in green streetscape design, adding beauty and function. Additional infiltration can be achieved through the use of pervious paving materials for sidewalks and pathways.





Allow Pervious Parking Lots - Conventional paved surfaces, including streets, sidewalks, and particularly parking lots, typically utilize asphalt, concrete, or tar surfaces that are entirely impervious. The result is more rapid, heated and polluted stormwater reaching watercourses. There have been many recent innovations in materials and pervious pavement systems that allow water to infiltrate large- and small-scale paved surfaces. In New England, pervious surfaces face the challenge of frost and clogging from salt and sand. However, different materials and applications continue to be tested successfully. Coupled with pervious paving, infiltration planters with shade trees should be applied in surface parking lots to further enhance the area’s ability to manage and treat stormwater.

Infiltration Parks - This form of bio-retention is combined with useable public or private open spaces while allowing rain water to be temporarily captured and stored for a short time, cleaning stormwater runoff before infiltration. Open spaces are integrated into the stormwater system through grading so that the rain water flows towards the open space. Creating a gradual and imperceptible depression allows water to collect in the open space and stay there long enough to infiltrate without giving the appearance of a stormwater facility.

Plant Shade Trees - The broad use of shade trees planted around Wellesley is a simple and beautiful solution to reducing heating and cooling needs for centers, corridors and neighborhoods. Shade trees should also be planted in public and private parking areas. When planted along the south and west sides of buildings, shade trees drop their leaves during the cooler fall season allowing warming sunlight to reach sidewalks and buildings. Additionally, they create a lush canopy in the warmer months providing shade to keep the sidewalk and adjacent buildings cool. Shade trees provide a low tech, low cost solution to reduce energy needs, absorb stormwater, cool parking lots, and add aesthetic value to a development site.

Improve Lighting, Landscaping, and Screening Standards - Some public and private parking lots in Wellesley have little or no landscaping or lighting. Adequate lighting

levels and appropriate fixtures provide an important safety feature for pedestrians and vehicles, and an added sense of security. Along with shade trees, certain ground covers can enhance stormwater treatment and aesthetic quality. Additionally, the combination of certain conifers, shrubs and fencing materials can provide adequate screening from adjacent neighborhoods. The trade-off would be that installing landscaping, lighting and screening enhancements will require the reuse of some parking spaces which is acceptable if there are excess parking or nearby parking that can be substituted.

PARKING LOT LANDSCAPE TREATMENTS	
	
<i>Private Parking Lot Landscaping</i>	<i>Rear Landscaping and Access from Parking Lot</i>
	
<i>Shade Trees in Parking Lot</i>	<i>Solar Panel Shade Parking Lot with Car Charging Station</i>

Additional New Off-Street Parking Regulations and Standards.

- Revise off-street parking requirements table from zone and building-specific, to use-specific.
- Provide new standards to accommodate mixed-uses within certain districts.
- Provide new standards to allow for off-street parking reductions based on shared parking, mixed use, satellite parking, availability of public parking, proximity to alternative transportation modes, TDM programs, and other factors.

- Provide new standards to manage parking supply efficiently through facilitation of shared parking, shared access, and internal access and connectivity. Shared parking should be permitted in each district either by right or special permit as an alternative to creating a “parking overlay districts” in certain districts.
- Revise applicability standards for expansion, alteration, reconstruction and other changes to existing parking facilities and property uses.
- Ensuring that new and alternative parking standards harmonize with Wellesley’s historic village and neighborhood character by enhancing landscaping of existing parking where possible.
- Establish regulations for loading areas/dumpster placement.
- Establish minimum/maximum sizes of on-street parking spaces.
- Provide new regulations for structured parking including decks, garages, and tuck-under facilities.

Designate Parking Improvement Implementation Team and Goals.

In order to effectively proceed with the implementation of the following recommendations, the Town should appoint a Parking Task Force with representatives for various town departments, boards, and representation from business, neighborhood and civic organizations. The Task Force should work together to prioritize recommendations, establish an implementation schedule, and parking goals. For example, a Parking Availability Goal should be established for public on-street and off-street parking areas. Generally, parking is at effective capacity when 85 to 90% of the spaces are occupied. Therefore, a consistent 15% parking availability in on-street and off-street facilities should be a goal. Where parking availability is not reaching this goal, additional public or private parking spaces should be targeted for the immediate vicinity.

Evaluate Utilization and Management of the Public Parking System.

Consistently Monitor Public Parking Occupancy and Turnover.

In order to determine a Parking Availability Goal of 15% the Town should consistently monitor public parking (on and off-street) occupancy. This might be carried out by parking enforcement staff which is most familiar with on and off-street inventories and is making regular passes through these facilities. These occupancy counts should be enhanced periodically with turnover counts which may determine areas where commuters, employees, and other long-term parkers are conflicting with designated short-term shopper parking spaces.

Evaluate Public Parking Meter Rates, Timing and Enforcement

Evaluate Time Limits - The Town should determine how effectively time limits on parking meters allow for longer-term parking away from village core areas and shorter-term parking closest to shops to create a business-friendly approach to parking that encourages turnover. Increasing on-street turnover while providing employee parking at appropriate remote locations is the overall goal. The timing of on-street and off-street parking limits should be re-evaluated with direct input from property owners, business owners, and employees. Wellesley's metered parking is an effective way to enforce time limits and ensure better turnover of short and long-term spaces. Meter revenues are also typically used to support parking enforcement and to improve/expand public parking in the village and neighborhood centers. However, local businesses are often opposed to parking fees as they may be perceived as driving customers away. One possible consideration would be to allow the 15-30 minutes of parking for free in the shorter term "front of store" spaces before the meter begins to count the time. This can be a much appreciated convenience for both merchant and customer while continuing to improve the turnover of short-term parking spaces.

Evaluate the Permit Program - The permit program is intended to encourage residents and employees to park in more remote locations away from the core area of Wellesley's village and neighborhood centers where customer parking is critical to business success. The Town should consider promoting the expansion of the program with the goal of providing incentives for employees to park further off the primary streets.

Evaluate Parking Enforcement - Business and property owners benefit by consistent parking enforcement in village and neighborhood centers to ensure that the limited number of spaces, including the use of short and long-term spaces, are most effectively utilized. The general goal is to encourage more employees to park in the public off-street lots.

Make Improvements to the Public Parking System.***Plan for Long-Term Strategic Parking Expansion.***

There are certain locations in the five study areas where a shortage of parking is apparent. The specific recommendations for these areas (see below) identifies potential locations for additional public on-street parking, off-street surface parking, and structured parking. In these locations, the Town would have to work with private property owners to assemble enough additional space to expand public parking. This may involve the acquisition or lease of private property, or a public/private partnership

redevelopment. In the long-term, the Town should consider structured parking in a strategic and central location, possibly in coordination with a mixed use development that may occur as commuter rail service and facilities are improved. Specific locations where this may be appropriate include:

- Cameron Street Parking Lot
- Railroad Avenue Parking Lot
- Tailby/Wellesley Square MBTA Station Parking Lot
- Undetermined location in Wellesley Hills near commuter rail station

An evaluation of opportunities for structured parking (decks, garages, and tuck-under facilities) should consider parking utilization, potential locations, design and costs. Because the per space cost of constructing a new parking garage and deck are significantly greater than surface parking, projected occupancy rates, fee structures, and funding mechanisms need to be carefully considered.

Establish a Public Parking Fund.

The new parking zoning bylaw should allow for payment in lieu of parking in the village and neighborhood centers (or Business and Commercial Districts). While not currently in place in Wellesley, the Town should consider forming a public parking fund. It is suggested that the fund be defined broadly so that the Town can use collected fees for any capital investment that adds new parking spaces, reconfigures lots to improve the utilization of existing spaces, closes or consolidates curb cuts, improves the management of supply or demand for parking spaces, adds new or improved pedestrian paths or amenities, provides bicycle parking or public transit facilities, or encourages other measures that shorten perceived walking distances in the village and neighborhood centers. A Public Parking Fund would be one of many techniques that can be employed to finance public parking structures and lots. Direct expenditures and bonding using parking revenues or tax increment resources are other common methods.

Consider Options to Enhance Parking Management Program.

Effective parking management might be best advanced through a formalized plan involving the Town, MBTA, and business and property owners. The purpose of this plan would be to formalize cooperative strategies to carry out physical improvements, improve signage and access, maximize efficiency through shared uses, and make appropriate policy changes regarding managing and enforcing the parking supply. One option is to create a Parking Management Authority to formally organize shared

interests in the supply and management of parking. The authority would be in a position to explore many methods to improve parking access, signage, shared use, and supply. They would report regularly to the Town officials regarding the results of their efforts including several of the above recommendations.

Facilitate Shared Parking on Public and Private Parking Lots.

Allow and Encourage Shared Parking.

One of the keys to success for village and neighborhood centers (or other mixed-use districts) is the ability for parking spaces to be shared among different users at different times. Not only does this promote the efficient use of the land, it supports the type of active pedestrian district that is critical to supporting vitality and economic opportunity in these centers. Participants can be any combination of private property owners, institutions, or the municipality. The ability for parking spaces to be accessed by different users (residential, civic and commercial) at different peak times is a key to success for mixed-use districts such as Wellesley Square.

Shared parking accommodations can be accomplished through new parking zoning regulations as well as formal agreements, but need to be planned and managed. Formalized parking agreements allow participants to rationalize and improve shared parking opportunities including access for cars or pedestrians that create more efficient use, layouts and access.

Improve Parking Lot Connectivity.

On-site observations indicate that private parking efficiency could be improved in several places in Wellesley. The Town should consider incentives for private property owners to coordinate and provide internal connection where possible which could result in the creation of additional parking spaces and reduction in curb-cuts.

Recommendations Specific for Parking Study Areas.

Study Area 1: Wellesley Square

Wellesley Square is the heart and soul of the community. It includes a broad range of commercial, residential and civic uses in a traditional New England village development pattern. It is also accessibly by train, bus, bicycle, and on-foot in addition to private vehicles. There are significant opportunities for mixed use and

transit oriented development that can be supported by parking improvements as identified below:

Wellesley Square Commuter Rail Station - The Tailby parking lot fills up quickly in the morning, especially the long-term spaces. This leaves local commuters unsure if they will be able to find parking. Currently, there are no programs that give preferential parking to Wellesley residents at station lots. The Town should consider reserving an adequate number of parking spaces for local commuters (e.g., the Town of Concord has done this with MBTA stations in West Concord and Concord Center). The Town should also consider the possibility of creating a transit-oriented development (TOD) on the Tailby lot, coupled with structured parking and a pedestrian connector that could serve the Wellesley Square and Linden Street districts, as well as local residents and commuters.

Parking Meters - Most lots and all on-street public parking in Wellesley Square use individual coin-op meters. Over time, the Town should consider replacing this equipment with electronic meters and pay stations that can accept credit cards and parking passes as well as coins. The equipment replacement cost for individual meters would likely be considerable. However the electronic pay stations which can be used for both on-street and off-street parking would be more cost effective over time and efficient in terms of collections. Electronic equipment would also provide data on parking occupancy and turnover to the Town could monitor the efficiency of the public parking system by area.

Additional Recommendations:

- Consider options for structured parking coupled with development on the Waban Street parking lot and Railroad Avenue parking lot. This could possibly be accomplished as a public/private partnership.
- Consider option of new satellite parking lot coupled with development on the vacant Weston Road Substation. This could possibly be accomplished as a public/private partnership.
- Increase enforcement of parking regulations and restricted turning movements into and out of parking lots.
- Improve wayfinding to all public parking lots as well as to private spaces located on the north side of the square and accessed by the driveway at Central Street across from Cross Street.

Study Area 2: Linden Street

Parking appears to be ample and generally available along the Linden Street corridor except on certain peak events. The parking spaces at Roche Brothers supermarket exceed the current dimensional standard width (9 feet instead of 8) which works well for a supermarket with grocery carts. Motorists can comfortably enter spaces. Parking recommendations to improve walkability and enhance development opportunities include the following:

- Explore the possibility of additional connections to Wellesley Square, using a pedestrian overpass and/or parking deck/structure on Tailby and Railroad Avenue parking lots.
- Enhance the pedestrian environment by the following:
 - Improve and expand connections between parking lots and from the public sidewalk to the store entrances.
 - Reduce excess asphalt by installing new shade trees.
 - Encourage new infill development on the corners and along frontages to improve connections to the sidewalks and create a sense of enclosure.
 - Require parking to be placed to the rear of the building's front façade.
- Provide standards for traffic control (signage/stripping) in parking lots (e.g. stop sign is not provided at Roche's driveway).
- Continue to retain the 8 on-street parking spaces along the north side of Linden Street but consider removing the meters. While these spaces do not appear to be well-utilized, they provide convenient short-term parking and a buffer between vehicles and the pedestrian.

Study Area 3: State Street

Recommendations for the State Street area are as follows:

- Whole Foods has become as significant draw to this area. The Town should meet with the company to see if some of the narrow spaces (8 feet) closest to the store could be reconfigured in 9 feet wide, depending on overall parking capacity and availability. Additionally, the electric vehicle charging spaces are one of the sustainability standards being considered for the new parking bylaw. However, these spaces appear to be regularly occupied by gas vehicles.
- Encourage shared parking at private lots and on the parking lot behind Needham Bank. This lot is publically owned and may provide an opportunity for a parking

deck that could be built in partnership with new development where appropriate and desirable.

- Consider opportunities for redevelopment at the Haskins Automotive site.
- Encourage shared curb cuts and internal parking lot access and circulation.
- Provide a safer pedestrian crossing over the railroad tracks.

Study Area 4: Wellesley Hills

Public parking in Wellesley Hills is limited to 52 spaces at the MBTA Station and 62 spaces at Eaton Court. However, the broad range of different land uses provides good opportunities for shared parking. Given the scale of existing and recent development in the area, the Town should plan for larger parking demand uses in the future.

Recommendations are as follows:

- Explore opportunities for a public/private partnership for additional development and parking near the Fire Station. Give consideration to attracting a mixed use TOD development with an “anchor” retail establishment, such as a grocery store.
- Improve wayfinding to Eaton Court and private lots.
- Work with property owners where shared parking is possible such as the Unitarian Church.
- Enforce, re-sign, or reevaluate the peak hour parking restriction and additional travel lane.
- Address the unsafe pedestrian crossing at the Route 9/Route 16 intersection.
- Expand public parking by adding parking meters on Cliff Road.
- Identify opportunities and locations for additional commuter parking for users of the Wellesley Hills MBTA station. This could include TOD with structured parking through a public/private partnership.
- Enforce parking lot landscape codes.

Study Area 5: Lower Falls

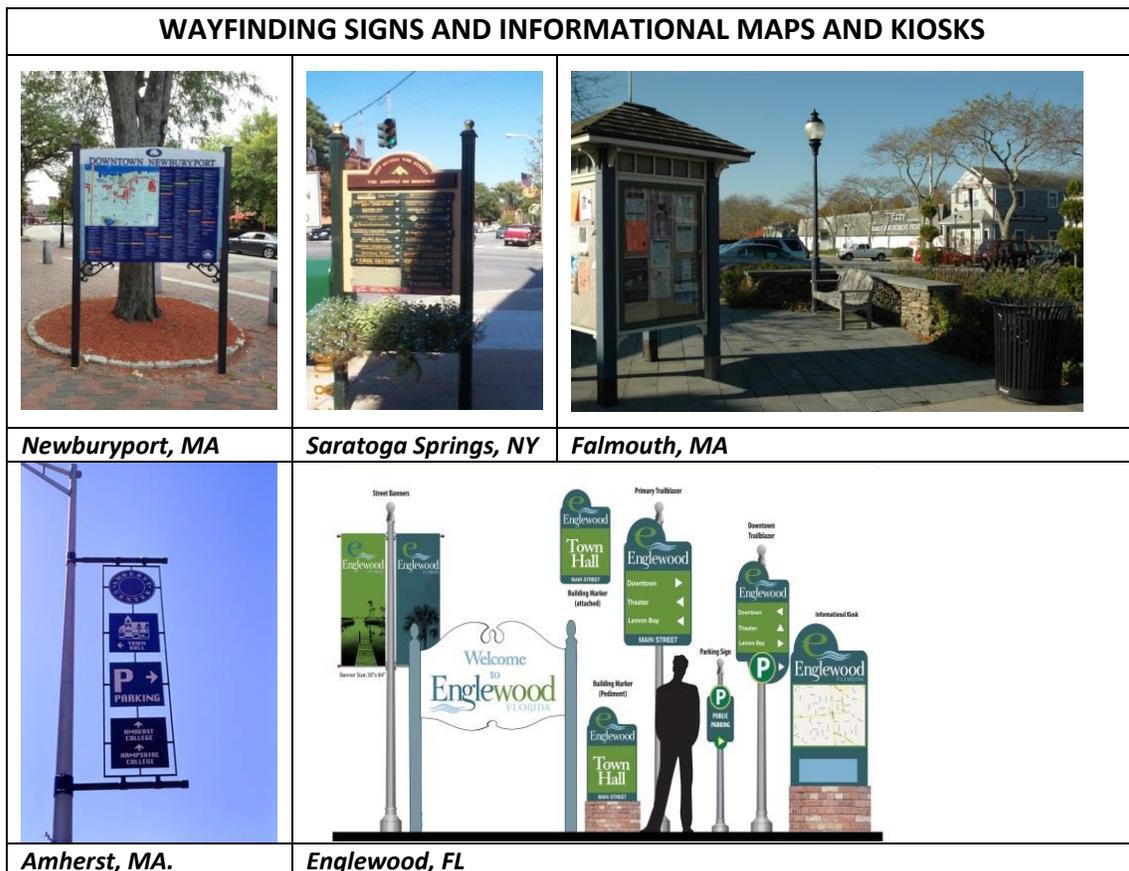
Presently there are only 82 on-street and off-street parking spaces in Lower Falls. However, there are significant areas dedicated to private parking that are underutilized and provide an opportunity for infill development. Recommendations are as follows:

- Encourage property owners to coordinate design, layout, connectivity, access, and shared use of individual parking lots to create efficient utilization and opportunities for infill development (e.g. One Washington Street).

- Identify opportunities and locations for additional public parking to support economic development including structured parking through a public/private partnership.
- Improve wayfinding for off-street parking (public and private).
- Consider improvements to the peak hour on-street parking ban in terms of signage, enforcement and general safety.

Install Wayfinding Signage and Parking Maps.

Attractive entrances or “gateways” and wayfinding sign systems into village and neighborhood centers are critical to making a good impression on residents, visitors, students, and commuters (all potential customers). Gateway treatments and wayfinding signage also function as traffic-calming devices, providing a message to travelers to slow down as they approach the center.



While there is some directional signage in Wellesley Square and along the primary corridors, it is not as visible and intuitive as it should be to effectively indicate to travelers in cars and on foot that public parking is available around the center. New

wayfinding signage should be installed that is coordinated, attractive, and uniform in design. It should be strategically located and visible at entry points and lead visitors to public parking lots as well as other points of interest such as the commuter rail station, Town Hall, Public Library, Post Office, local businesses, and others.

In addition to wayfinding signage, information maps and kiosks should be strategically located around village and neighborhood centers. The kiosks can be both stand-alone structures and wall-mounted units and should contain a business directory map, points of public interest, public and private parking areas, and information on upcoming events and activities in Wellesley.

Improve Multi-Modal Access to Village Centers, Neighborhoods and Commercial Corridors

The Town recently completed the Wellesley Walks Pedestrian Plan as a guide to making capital improvements to the sidewalk network as well as low cost/high impact enhancements to the pedestrian environment throughout the community. In addition to pedestrian facilities, other alternative access modes to the private automobile should be continually evaluated and enhanced such as:

- Bicycle facilities - location of multi-purpose paths, corals, and racks.
- Commuter rail - service improvements and connectivity to the centers.
- Bus transit - service improvements, stops, pull-outs and shelters)
- Car share programs - allocation of spaces on public and private lots.

Bicycle and pedestrian access throughout Wellesley is critically important in order to support the businesses, civic, and institutional uses. Desire lines (unpaved areas, disturbed by bicycle and pedestrian traffic) often pass across private property. Where appropriate, the Town should work with property owners to establish access easements that ensure that property rights are protected but allow for non-motorized passageways and associated improvements.

5.0 NEW OFF-STREET PARKING ZONING BYLAW

HSH has prepared a new, draft off-street parking zoning bylaw, as follows, that incorporates provisions consistent with the Town's goals, including facilitation of appropriate development, redevelopment and reuse of properties in existing commercial corridor, village, and neighborhood districts. New and alternative provisions address the following Town objectives: limited existing on-site and off-site parking; simplification of the parking provisions; efficient land use and shared parking options; environmental stewardship with regards to constructions standards, maintenance, screening, landscape, storm water drainage, and charging stations; and support for multi-modal transportation options including public transit, pedestrian and bicycles.

The Planning Board anticipates using this draft bylaw as a framework and guide, to aide in the development of a revised bylaw for consideration at a future Town Meeting.

SECTION XXI. OFF-STREET PARKING (DRAFT 2)

A. PURPOSE AND INTENT.

In order to minimize traffic congestion, air pollution, and the risk of motor vehicle and pedestrian accidents, as well as to promote other elements of sound community planning, off-street parking, loading spaces, circulation, and access shall be required and designed to protect or enhance the desired character of a given center, neighborhood or corridor. It is further the intent of this bylaw to:

1. Promote a “park once” environment that will enable people to conveniently park and access a variety of commercial and civic enterprises in pedestrian friendly environments by encouraging shared parking.
2. Ensure there are adequate parking and loading facilities to serve the use or uses of the property;
3. Ensure that parking facilities are designed to provide proper circulation, reduce hazards to pedestrians, and protect the users of adjoining properties from nuisance caused by the noise, fumes, and glare of headlights which may result from the operation of vehicles parking off the street;
4. Reduce congestion in the streets and contribute to traffic safety;
5. Encourage alternate modes of travel that will reduce dependence upon the single occupancy automobile;
6. Reducing fragmented, uncoordinated, inefficient, single purpose parking;

7. Avoid adverse parking impacts on neighborhoods adjacent to redevelopment areas;
8. Providing flexibility for redevelopment of small sites; and
9. Encouraging low impact development (LID)/light imprint development techniques such as previous paving and natural infiltration systems, where appropriate.

B. ADMINISTRATION.

1. Permits.

Upon the filing, by the owner of a lot or by the operator or occupant thereof with the consent of the owner, of an application for a permit accompanied by a parking plan complying with the provisions of Subsection C below:

- a. for the construction, enlargement, or alteration of a parking or storage area;
- b. for the erection, enlargement or substantial alteration of any building for which parking would be required by this Bylaw; or
- c. for a change in the use or uses that would require different requirements from those applicable to the former use, the Building Inspector shall determine whether such plan is in compliance with the provisions of this Bylaw and if so he shall issue a permit therefore. If the Building Inspector determines that the plan is not in compliance with this Bylaw he shall deny the application in writing setting forth his grounds for denial.

2. Special Permits.

An applicant who proposes to erect, enlarge or substantially alter a building, for which parking is required by this Bylaw, which parking to be provided is insufficient, may apply to the Special Permit Granting Authority for a special permit in accordance with this SECTION XXI. and SECTION XXV. of this Zoning Bylaw subject to the following:

3. Applicability.

- a. *New Buildings and Structures.* No new building or structure shall be constructed or used, in whole or in part, and no building, or part thereof, shall be altered, enlarged, reconstructed or used, and no land shall be used unless there is provided off-street parking in accordance with the following conditions:
 - I. A parking plan submitted in accordance with Subsection B.1, shall have been approved by the Inspector of Buildings or the Special Permit Granting Authority as provided in SECTION XVIA.

- II. No existing off-street parking spaces shall be eliminated by the replacement or enlargement of an existing building or structure, unless replaced by spaces provided in accordance with this Section.

b. *Existing, Non-Complying, and Expanded Uses.*

- I. **Existing Structures** - Any structure or land use lawfully in existence prior to the adoption of this bylaw shall not be subject to the requirements of this Section so long as the kind or extent of use is not changed, and provided further that any parking facilities now serving such structures shall not in the future be reduced below such requirements.
- II. **Change or Expansion of Use** - Whenever there is an alteration or conversion of a structure or a change or expansion of a use which increases the parking requirements, the total additional parking requirements for the alteration, conversion, change, or expansion shall be provided in accordance with the requirements of this Section. A waiver may be requested under the provisions of Subsection B.2.
- III. **Non-complying Commercial Structure** - Where the computed parking requirement for a nonresidential use in any Business or Commercial District is five (5) spaces or less, the Planning Board may waive all or part of such computed requirements.
- IV. **Non-complying Residential Structure** - Where additions or conversions to existing structures add living space but do not add dwelling units, and such existing structures do not meet the parking standards of Subsection D, one (1) parking space shall be provided for each additional room. Single detached dwellings shall be exempt from this requirement.

C. PARKING PLAN

No parking or storage area (whether required by this Bylaw or otherwise subject to Subsection G of this Section shall be constructed or altered; no building permit for the erection, enlargement or substantial alteration of any building for which a parking or storage area would be required by this Bylaw shall be issued; and the uses to which a lot is put shall not be changed to a use or uses requiring different parking requirements from those applicable to the former use; unless in each case a permit has been issued in accordance with the provisions of Subsection B.1 of this Section based on an Off-Street Parking or Storage Plan.

1. **Plan Requirements.** The plan shall show such parking or storage areas and/or the parking or storage areas associated with such buildings or changed uses. Said Off-street Parking or Storage Plan shall include:
 - a. The quantity, location, and dimensions of all driveways, maneuvering spaces and aisles, parking spaces, storage areas, and drainage facilities;
 - b. The location, size and type of materials for surface paving, curbing or wheel stops,

trees, screening and lighting;

- c. The location of all buildings and lot lines from which the parking lot must be set back, and
- d. Such other information as the Building Inspector may reasonably require. The plan shall be a drawing at a scale of 1 inch equals 20 feet or 1 inch equals 40 feet or at such other scale as the Building Inspector may direct.

2. **Supplemental Information.** Where necessary for the administration of this Section, the Building Inspector may require that the owner, operator or occupant of a lot or any building thereon, furnish a statement as to the number of employees customarily working at any one time on the premises. The Building Inspector may, at any reasonable time, enter upon a lot or into any building thereon, in order to make such determinations as are necessary for the administration of this Section.

D. REQUIRED PARKING BY USE.

In all districts which require off-street parking in accordance with this bylaw, off-street parking shall be provided for uses according to the following table. Where no requirement is designated and the use is not comparable to any of the listed uses, parking requirements shall be determined by the Planning Board upon a recommendation by the Building Inspector based upon the capacity of the facility and its associated uses.

OFF-STREET PARKING REQUIREMENTS	
USE	MINIMUM NUMBER OF PARKING SPACES
Residential	
Any Dwelling Unit, except as noted below	1 space/500 square feet of gross floor area, up to a maximum of two per dwelling unit
Group Dwelling for elderly and people with disabilities, boarding house, dormitory, SRO, shelters, and halfway house	1 space/1,000 square feet of gross floor area, up to a maximum of one per bedroom
Hotel, Motel, Inn, Bed-and-Breakfast (See restaurant entry for associated restaurants which are open to non-guests.)	1 space/establishment, plus one per sleeping room, plus one per 400 square feet of meeting rooms
Public Assembly	
Theater, Gymnasium, Auditorium, Place of Worship, Club/Lodge or similar place of public assembly, with seating facilities	1 space/3 seats of total seating capacity
Hospital (excluding medical offices and uses which are not part of the hospital definition)	1 space/500 square feet gross floor area
Kindergarten to 12th Grade Schools, YMCA, Community Recreation Facility, Library, Museum, Funeral Parlor, and Country Club	1 space/400 square feet of gross floor area

College, Business, Trade, or Industrial School classroom, laboratory, and other teaching areas	1 space/200 square feet of gross floor area (no parking required for on-campus auditoriums)
Indoor Recreation	1 space/200 square feet of gross floor area
Outdoor Recreation	20 spaces or 1 space/3 fixed seats, whichever is greater
Commercial	
General Commercial, Retail, Personal Service, Professional and Business Offices, including Banks, Insurance and Real Estate Establishments	1 space/300 square feet of gross floor area
Restaurants/Takeout (establishments selling foods prepared on premises, where consumption is primarily off the premises)	1 space/1.3 seats of seating capacity, plus one per 100 square feet of kitchen and waiting areas
Restaurant/Sit-Down, Bars, Taverns, and Nightclubs	1 space/2 seats of total seating capacity
Medical and Dentist offices, and transportation terminal	1 space/200 square feet of gross floor area
Home Occupation	1 space/employee
Animal Shelter and Animal Clinic	1 space/250 of gross square feet
Garden and Farm Supply/Nursery	1 space/1,000 gross floor area plus 1 space/2,000 square of land area
Vehicle Sales and Service	
Automobile Retail, Sales, Rental, Service, and Wash, and non-auto retail and service establishment utilizing extensive indoor and outdoor display areas	1 space/800 square feet of gross floor space, including outdoor display areas
Industrial	
Manufacturing, Industrial, Utility, Warehouse, Storage, or Wholesale Establishment (calculate associated office with office use)	1 space/1,000 square feet gross floor space
Transportation Terminal	1 space/200 square feet of gross floor area
Mobile	
Food Venders, Food Trucks, Farmers Markets and other temporary and seasonal uses in unheated outdoor space in any business or industrial district	None required
Mixed Use	
	Sum of various uses computed separately

E. PARKING LIMITATIONS.

- 1. Limits on Surface Parking** - Surface parking shall not exceed 100% of the required minimum parking in the table of OFF-STREET PARKING REQUIREMENTS in Section D. Parking in excess of the required parking limitation of this section may be granted by

Special Permit from the ZBA pursuant to a demonstrated need by the applicant and that the following additional review criteria are sufficiently addressed:

- a. The need for additional parking cannot reasonably be met through provision of on-street parking, shared parking with adjacent or nearby uses, or other permitted off-site reduction methods;
 - b. The proposed development demonstrates that its design and intended uses will continue to support high levels of existing or planned transit and pedestrian activity; and
 - c. The site plan indicates where additional parking can be redeveloped to a more intensive transit supportive use in the future.
2. **Limits on Structured Parking** - Spaces provided within the footprint of a structure containing one or more other uses, including rooftop, at-grade, or below grade spaces shall not be limited by the maximum off-street parking requirement above, provided the square footage footprint dedicated to structured parking is less than 50% of the total square footage footprint that would be required for surface parking. See Subsection G for additional structured parking standards.
3. **Exemption for Outdoor Dining** – Permitted outdoor dining located on the same site as the primary use and building shall be exempt from the calculation of required off-street parking spaces.

F. PARKING REDUCTION METHODS.

1. **Shared Parking.**

By Special Permit, the Planning Board may determine that a proposed development consisting of two or more uses located on a single parcel or adjacent parcels will generate different hourly, daily and/or seasonal parking demand due to the varied hours of operation of each use and frequencies of customer and employee occupancy of available parking spaces. Under these criteria, the Planning Board may approve a site plan utilizing shared parking.

- a. **Required Information.** The applicant shall provide a parking space count and justification analysis. Approved shared parking requires the present construction of a lesser number of spaces, provided that the applicant shall provide the Planning Board with a shared parking plan which shall be based upon the following:
 - I. A complete and accurate description of the proposed uses, floor areas devoted to such uses, the number of seats or rooms assigned to each use, and the days and hours of operation of each use for each day of the week and for any seasonal variations in operations.

- II. A complete and accurate description of the number of employees and their respective shifts.
 - III. A complete and accurate description of the projected total peak hour or daily period of occupancy by patrons for each proposed use during weekdays, Saturday, Sunday and any seasonal variations in peak hour or period of occupancy.
- b. Base Calculation of Shared Parking Space Reduction. In all cases except as provided in Subsection 1.c below, at least 66% of the number of required parking spaces shall be provided.
 - c. Alternative Calculation of Shared Parking Spaces Reduction. As an alternative to Subsection 1.b above, the minimum number of parking spaces for a mixed or multiple use development where shared parking is proposed may be determined by an evaluation prepared by the applicant following the procedures of the Urban Land Institute (ULI) Shared Parking Manual (latest edition), the Institute of Transportation Engineers (ITE) Shared Parking Guidelines (latest addition), or other approved procedures determined by the Planning Board. A formal parking evaluation may be waived for small developments where there is established experience with the land use mix and its impact is expected to be minimal. If this evaluation determines that a different parking demand is generated than would be required under Subsection 1.b above, the lesser amount shall be required by the Planning Board.
 - d. Proximity of Shared Parking. Eligible shared parking spaces must be provided within 300 feet of the primary access for customers and 600 feet of employees.
 - e. Legal Agreement. A contract, agreement or suitable legal instrument acceptable to the Planning Board and Town Counsel shall be filed by the application which shall specify the location of all spaces to be jointly used, the number of such spaces, the hours during the day that such parking shall be available and the duration or limit, if any, on such parking.
 - f. Discontinuation. Nothing in this section shall relieve the applicant from providing parking facilities in accordance with this Section if subsequently the shared parking use agreement shall terminate. In such cases, the applicant shall return to the Planning Board to provide an alternative shared parking agreement or another method of relief acceptable to the Board.

2. Off-Street Parking Off-Sets and Reduction Methods.

- a. Special Permit Criteria. The requirements in Subsection D - the table of OFF-STREET PARKING REQUIREMENTS and Subsection F.1 above may be reduced by Special Permit from the Planning Board if an applicant can demonstrate to the

satisfaction of the Board that the required number of spaces will not be needed for the proposed use(s) and that fewer spaces meet all parking demands. Special Permit Review Criteria for such cases include:

- I. Use of a common parking area for separate uses having peak demands occurring at different times.
- II. Age or other characteristics of occupants which reduce their auto usage.
- III. The availability and reasonable proximity to public parking or transit.
- IV. The availability and reasonable proximity of long-term parking for employees.
- V. Peculiarities of the use that make usual measures of demand invalid.
- VI. The decrease in number of required spaces will not create undue congestion, traffic hazards, or a substantial detriment to the given business district or neighborhood, and does not derogate from the intent and purpose of this section.

Walking distance is measured along a designated sidewalk or pathway to the nearest entrance of the building, use, rail platform, or bus boarding location.

On-site parking reduction methods include the following:

- b. On-Street Parking Credit. All commercial and mixed use properties located adjacent to a public right-of-way where on-street parking is permitted may receive credit for one off-street parking stall for each 20 linear feet of abutting right-of-way for parallel parking. This provision shall be applied for on-street parking on the same side of the street as the proposed land use, or on the opposite side of the street if the property on that side of the street does not have the potential for future development. In considering credit for on street parking, all fractional spaces are rounded down. (meter time limits)
- c. Off-Site Public Parking Credit. On-site parking requirements may be reduced by up to 50% as determined by the Planning Board if an off-street public parking lot of 20 spaces or more exists within 300 feet of the principal use, and the public parking lot has ample spaces available to serve the immediate area as determined by a survey of peak hour occupancy and usage.
- d. Off-Site Employee Parking Credit. On-site parking requirements may be reduced by up to 20% if an off-site public or private parking facility is located within a 700-foot walking distance of the site and has sufficient capacity to provide long-term parking for employees. The applicant shall provide a lease or sufficient legal agreement to the Planning Board and Town Council indicating the long-term security of employee parking spaces.
- e. Payment to Public Parking Fund. In lieu of providing the total minimum on-site parking required, the Planning Board may accept a one- time payment per required parking space for all or a portion of required on-site parking that would be

- committed to a fund for the construction of public parking in the district. The Board of Selectmen shall establish the amount of payment required per parking space.
- f. Public Parking Reserve. In lieu of providing the total minimum on-site parking required, the Planning Board may accept a permanent easement on the property for the purposes of constructing public parking for all or a portion of required on-site parking spaces. The reserve easement shall be subject to review and approval by the Board of Selectmen.
 - g. Proximity to Bus Transit. The Planning Board may approve a 5% reduction for commercial uses and 10% reduction for residential uses in the minimum number of required on-site parking spaces where the main entrance is within a 400-foot walking distance of an existing bus stop and shelter providing both shade and seating. Walking distance is measured from the nearest entrance of the use to the bus boarding location.
 - h. Proximity to a Rail Transit Station. The Planning Board may approve a 10% reduction for commercial uses and 20% reduction for residential uses in the minimum number of required on-site parking spaces where the main entrance is located within a 2,640-foot (half-mile) walking distance of a rail transit station.
 - i. Car-Sharing Program. The Planning Board may approve a parking reduction by up to 5 spaces for each car-share vehicle where an active car-sharing program is made available to residents and/or employees; and where cars for the car-share program are available on the site or within a 700-foot walking distance of the site.
 - j. Carpooling/Vanpooling. A reduction of up to 5% of required parking, based on substantiated projections of reduction in parking demand, may be granted for any building or use exceeding fifty thousand (50,000) square feet of gross floor area that institutes and maintains a locally approved carpooling/vanpooling program. When permitted by the Planning Board, the applicant shall note on the plans the Carpooling/Vanpooling plan that the Planning Board allows for and any reasons for allowing or requiring the deviation and/or conditions of approval as deemed appropriate by the Planning Board. Allowed parking reductions may be terminated immediately upon failure of the owner, operator, tenant, or others responsible for such programs to maintain such programs in an ongoing and acceptable manner.
 - k. Shuttle Service. A reduction of up to 15% of required parking, based on substantiated projections of reduction in parking demand, may be granted for any building or use that institutes and maintains a continuous, personalized shuttle service. When permitted by the Planning Board, the applicant shall note on the plans the Shuttle Service plan that the Planning Board allows for and any reasons for allowing or requiring the deviation and/or conditions of approval as deemed appropriate by the Planning Board. Allowed parking reductions may be terminated immediately upon failure of the owner, operator, tenant or others responsible for such program to maintain such program in an ongoing and acceptable manner.

- l. *Transportation Mitigation Plans.* Transportation mitigation plans may be implemented in conjunction with any use but shall be required for all large scale parking facilities. Properties with a required number of parking spaces exceeding two-hundred fifty (250) spaces shall be classified large scale parking facilities. New construction of large scale parking facilities, or any change to existing uses that requires additional parking on site for large scale parking facilities, may require the applicant employ parking mitigation to reduce the number of surface parking spaces if determined appropriate by the Planning Board. Qualifying parking mitigation shall include but not be limited to: structured parking, parking shuttles, employee carpool plans, incentives for employees to use alternative transportation such as bicycles, telecommuting plans for employees, shared parking, and other similar plans. The Planning Board shall have the discretion to require parking mitigation for large-scale facilities as appropriate taking into consideration stormwater, surface water impairment, the character of the neighborhood, traffic, pedestrian safety, landscaping, and circulation.
- m. *Curb Cut Reduction and Internal Connectivity.* By Special Permit and at the discretion of the Planning Board, on-site parking requirements may be reduced if one or more of the following pedestrian safety improvements are made on site:
 - I. Permanently eliminates and/or significantly reduces the width of existing curb cuts in a manner that improves the pedestrian safety and access control on a public street; or
 - II. Provides a perpetual agreement for one or more driveway consolidations or interconnections that will alleviate traffic on a public street and facilitates shared use of off-street parking

3. Reserved Parking Areas

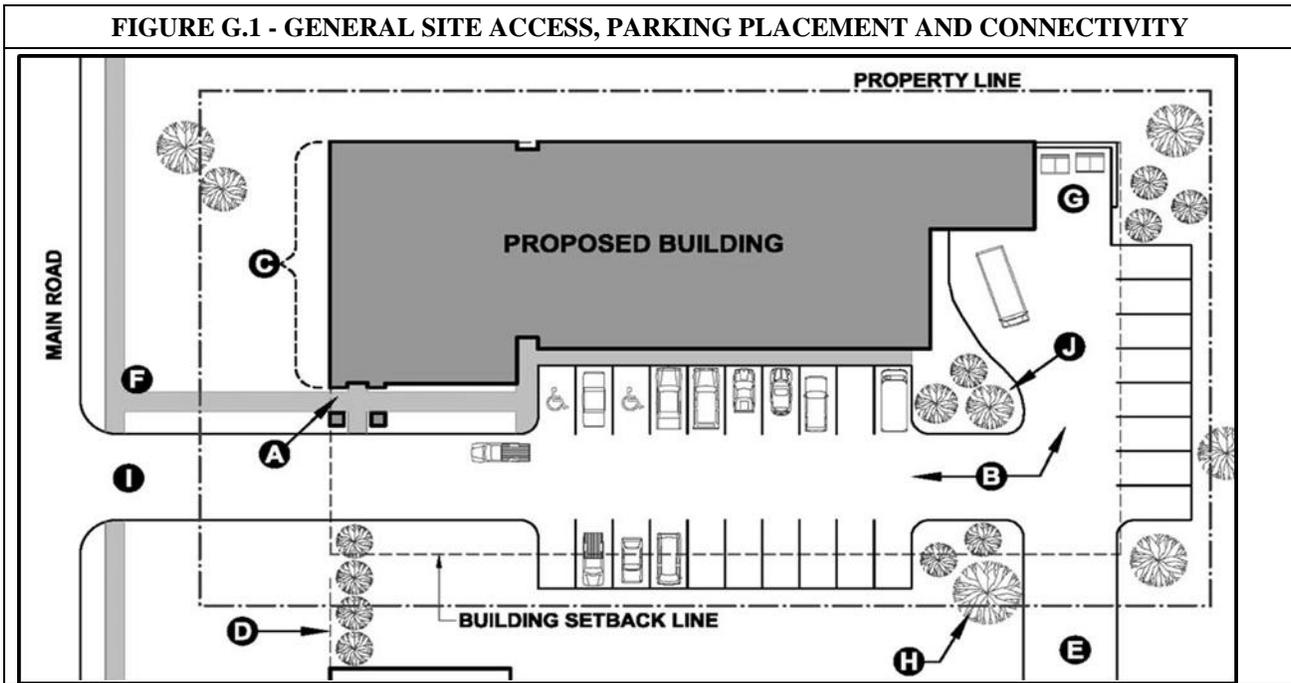
- a. The Planning Board may require that an area equivalent to the total amount or a portion thereof of the number of reduced on-site parking spaces be preserved and identified as “Reserve Parking” on the site plan.
- b. The parking reserve area shall be equal to 300 square feet per reduced space required by the Planning Board to be reserved on site and shall not be built on. The reserve parking spaces shall be properly designed as an integral part of the overall parking development, and in no case shall any reserve parking spaces be located within areas counted as yard setbacks.
- c. If, after one (1) year from the date of issuance of a certificate of occupancy, the Planning Board finds that all or a portion of the reserve spaces are needed, the Planning Board may require that all or any portion of the spaces identified as reserve parking on the site plan be constructed within a reasonable time period.

G. DEVELOPMENT STANDARDS.

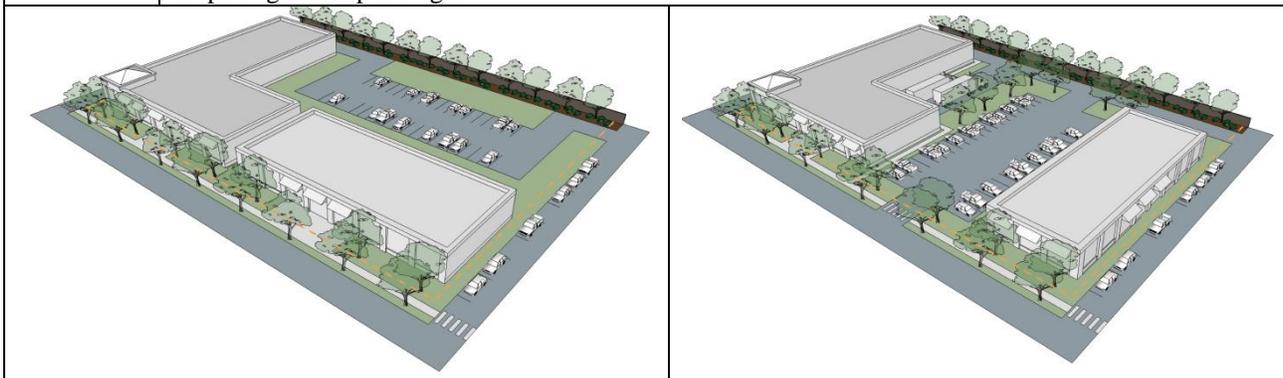
Each parking area hereafter devoted to the off-street parking of fifteen (15) or more vehicles regardless of whether said parking area is required by this Bylaw, shall comply with the standards as hereinafter set forth:

1. Parking Placement and Site Design

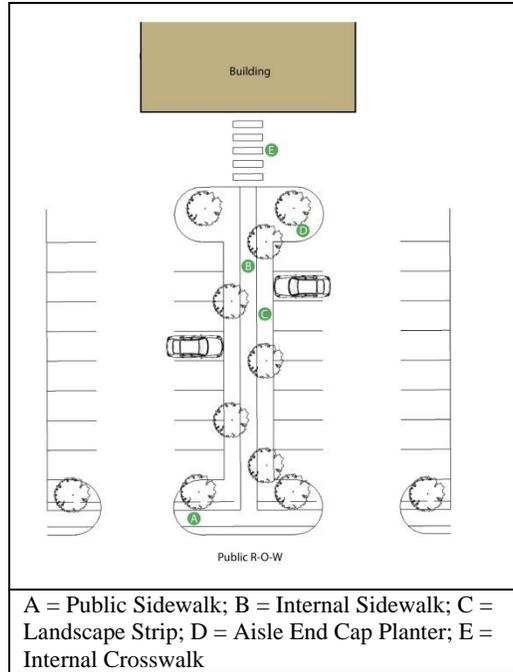
- a. *On-Site Parking Placement.* All off-street parking shall be located to the rear of the primary building or a minimum of five (5) feet behind the front wall elevation of the building nearest to the front property line and street right-of-way. A landscape buffer of at least five (5) feet wide shall also be installed to screen parking areas from the street. On-site parking placement shall be provided in accordance with the design standards and guidelines in Figure G.1 below.



General Design Guidelines.	
The building should be brought forward as close to the street as possible, with parking and other services tucked around the side and back. This helps to define the identity of the street and gives the business maximum public exposure.	
The parking area should be designed in regular, rectangular shapes. Irregular wedge shaped parking areas created by following angled property lines should be discouraged. Only the minimal amount of paved area necessary to meet the requirements should be provided.	
A.	The primary building entry should always be placed at the front of the building (facing the front lot line) and be clearly identifiable from the street. In this example, an entry vestibule is created which faces the front, side and rear parking area simultaneously.
B.	Parking should be placed at the side or rear of the lot and screened from view whenever possible. Where possible, break the parking areas up into groups of no more than 20 spaces per area (35 for large development projects) separated by landscaping areas.

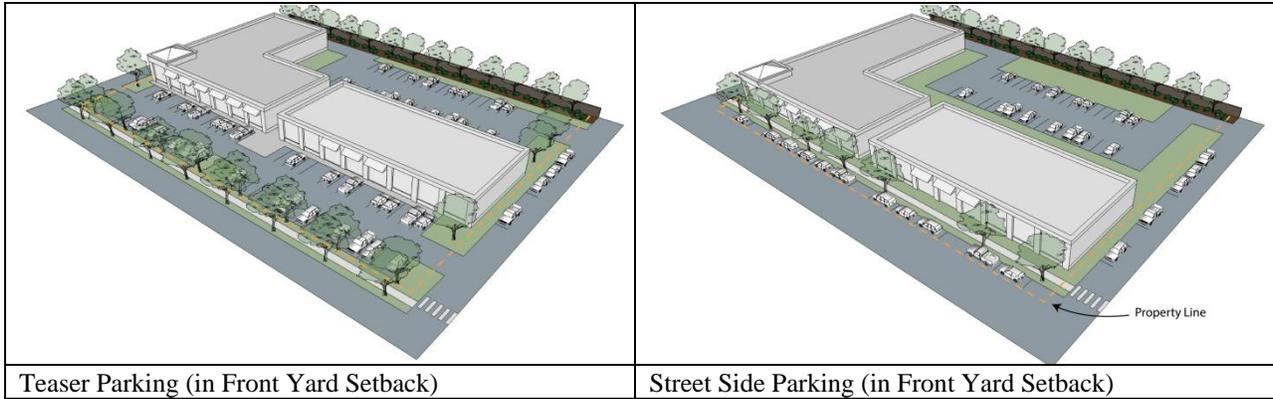
C.	Place as much of the building width at the front of the lot as possible to maximize front façade exposure to the traveling public. The front façade should be kept parallel to the street.
D.	To reinforce the “street-edge”, align with adjacent buildings which are also close to the front setback line. Landscaping can also be used to reinforce this line.
E.	Whenever possible, attempt to link with adjacent parking lots or provide shared parking areas which can serve neighboring buildings simultaneously. This provides a secondary means of access to the site and can ease congestion on the main road.
F.	Provide sidewalks for the full width of the property with a direct link to the primary building entry.
G.	Loading docks, service areas and trash facilities should be located at the rear of the building and not visible from the street. Fences, walls or landscaping can be used to shield them from view.
H.	Incorporate any existing, older trees into new site plan development whenever possible to reduce waste and utilize good shade trees.
I.	Minimize the amount of curb-cuts by having a single driveway in and out of the property from the main road whenever possible. Secondary access points from side roads are encouraged. Curb cuts should only be as wide as necessary to accommodate needed lanes. Curb radiuses should be kept to a minimum.
J.	Landscaped islands and other green space should be consolidated into useful areas, and not just narrow strips of grass or plantings.
	
<div style="display: flex; justify-content: space-around;"> Rear Parking Placement with Side Access Side Parking Access with Front and Side Access </div>	

b. *Internal Sidewalk Connectivity.* An internal sidewalk shall be provided for off-street parking lots with twenty (20) or more spaces. Internal walkways shall be provided in appropriate locations on the site and shall be clearly recognizable through the use of raised, textured or color surface treatments. Internal walkways shall connect to the public sidewalk where abutting the property and provide safe pedestrian access to the primary use entrance and on-site parking area.



c. Parking in Front Yard Area/Non-Residential and Mixed Use. By special permit, the ZBA may waive the requirement under Subsection 1.a above and permitted parking in the front yard under the following conditions:

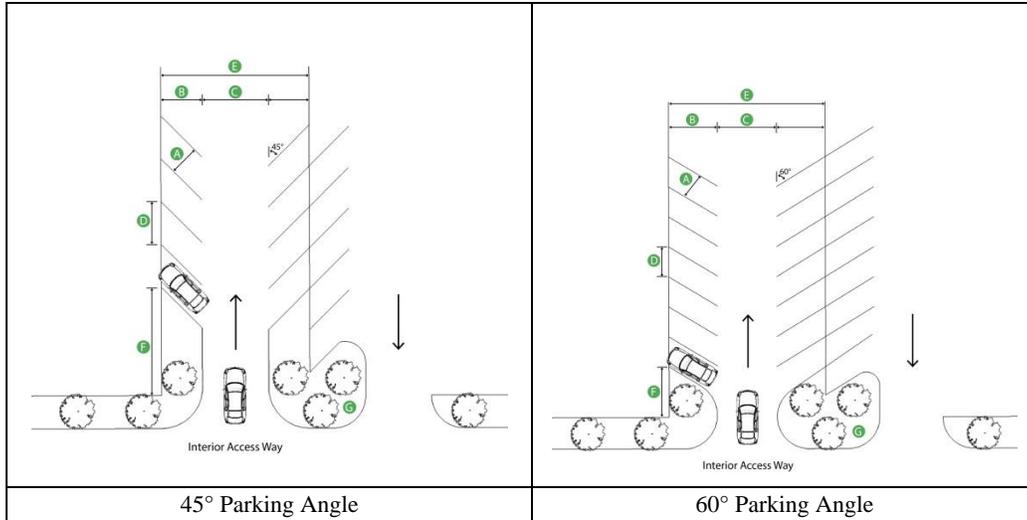
- I. **Targeted Rehabilitation Site.** A project involving the renovation of an existing building and site where parking currently exists in front of the building.
- II. **Teaser Parking.** In all districts except Single Residence, off-street surface parking may be allowed in front of the primarily building front elevation line if located a minimum of ten (10) feet from the street line, and screened with sufficient landscaping. In this case, the portion of the parking lot located in front of the primarily building shall be limited to one (1) double row of vehicles and associated parking aisle.
- III. **Street Side Parking.** Where not provided within the public street right-of-way and in Business and Commercial Districts, parallel or angled parking may be provided on a privately-owned lot directly adjacent to the public street right-of-way in combination with: a 5-foot planting strip (minimum) with street trees planted 40 feet on center; and a 5-foot minimum concrete sidewalk connecting extending to abutting lots and to the primary building on-site.



2. **Parking Aisle and Space Design.** Parking spaces and drive aisles must meet the following dimensions. Parking spaces and drive aisles using dimensions other than those specified may be used if approved by the Building Inspector.

DIMENSIONAL STANDARDS FOR PARKING SPACES AND AISLES								
Parking Angle	A. Stall Width	B. Stall Length	Stall Overhang	C. Vehicle Projection	D. Aisle Width		E. Maximum Width Curb-to-Curb	
					1-Way	2-Way	1-Way	2-Way
Parallel	8 ft	22 ft	0 ft	8 ft	12 ft	20 ft	31 ft	36 ft
45	9 ft	18 ft	2 ft	19 ft	15 ft	22 ft	49 ft	56 ft
60	9 ft	18 ft	2 ft	20 ft	14.5 ft	20 ft	54 ft	58 ft
90	9 ft	18 ft	2 ft	18 ft	20 ft	22 ft	58 ft	60 ft

Parallel Parking	90° Parking Angle



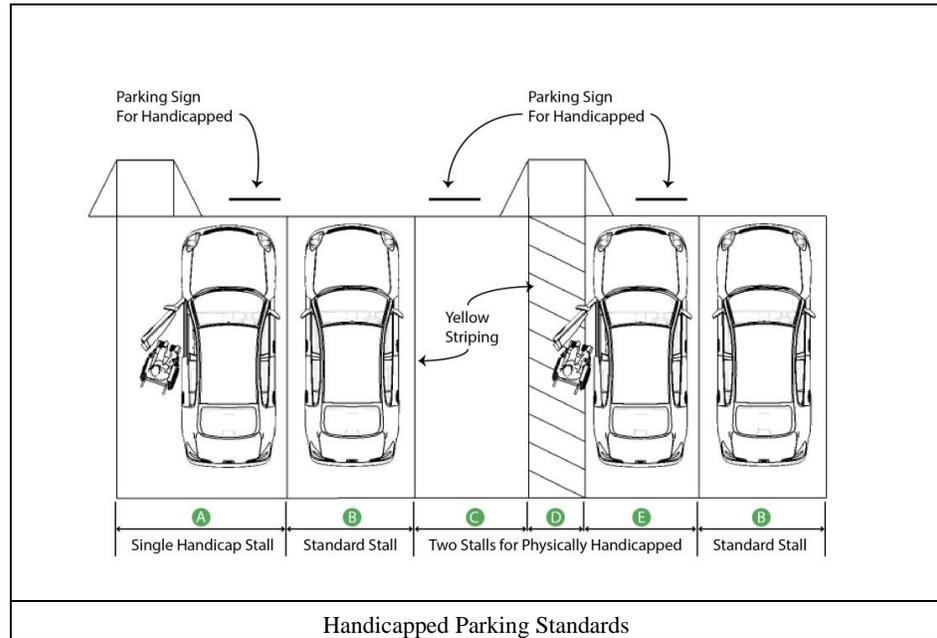
3. Special Parking Types and Standards

- a. Compact Cars. Compact car spaces shall have the minimum dimensions set forth in the following table and may be used to satisfy up to a maximum of 30% of the off-street parking spaces required. Such spaces shall be designated for "Compact Cars Only" by signs or pavement markings.

Angle of Parking	Width of Parking Space	Depth of Parking Space	Width of Maneuver Aisle
61° - 90°	7'6"	15'	24'
46° - 60°	7'6"	15'	18'
45°	7'6"	15'	15'
Parallel	7'0"	19'	12'

- b. Alternative Fuel Vehicles. In each parking lot or structure containing over 100 parking spaces, at least 2 spaces within the 10 spaces closest to the primary entrance to the building must be reserved for a hybrid or electric vehicle, and must have a sign indicating that reservation.
- c. Drive-Through Vehicle Stacking. A restaurant with a drive-through must provide at least 4 queuing spaces, measured from the first pick up window. A bank with a drive-through must provide at least 3 queuing spaces per drive-through lane, measured from the teller box or window. All other uses will be determined by the Building Inspector.
- d. Parking for Disabled Persons. Parking spaces for disabled persons shall comply with current the Americans with Disabilities Act guidelines and shall be at least eight feet

(8') wide with an adjacent access aisle at least five feet (5') wide. Parking access aisles shall be part of an accessible route to the building or facility entrance. Accessible parking spaces shall be designated as reserved for the disabled by a sign showing the symbol of accessibility. Painting of the paved area for the dedicated parking spaces alone shall not be sufficient as the sole means of identifying these spaces.



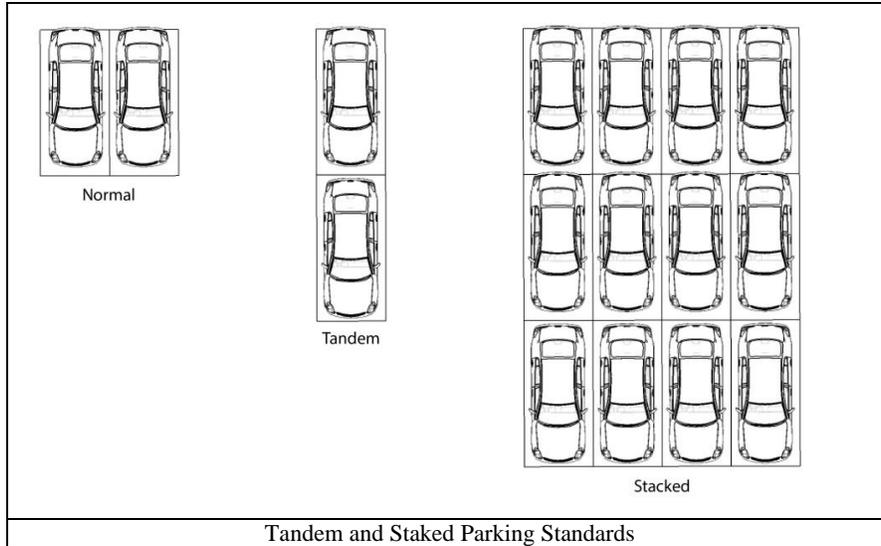
4. Parking Access

- a. The number of driveways permitting entrance to and for exit from a lot shall be limited to two per street line. Driveways shall be located so as to minimize conflict with traffic on public streets and where good visibility and sight distances are available to observe approaching pedestrian and vehicular traffic.
- b. The width of a driveway for one-way traffic shall be not less than twelve (12) feet as measured at its narrowest point. The width of a driveway for two-way use shall be a minimum of eighteen (18) feet and a maximum of twenty-four (24) feet, as measured at its narrowest point.
- c. All parking areas shall be so arranged and designed that the only means of access and egress to and from such areas shall be by driveways meeting the requirements of this Section.
- d. Driveways shall be arranged for the free flow of vehicles at all times, and all maneuvering spaces and aisles shall be so designated that all vehicles may exit from and enter into a public street by being driven in a forward direction.

5. Vehicle Maneuvering. Each required off-street parking space shall be designed so that

any motor vehicle may proceed to and from said space without requiring the moving of any other vehicle or by passing over any other parking space, except as follows:

- a. Stacked or Valet Parking. By Special Permit, the Planning Board may be allow valet or tandem parking if an attendant is present to move vehicles. If stacked parking is used for required parking spaces, a written guarantee must be filed with the Town ensuring that an attendant will always be present when the lot is in operation. The requirements for minimum or maximum spaces continue to apply for stacked parking.



- b. Tandem Parking. By Special Permit, the Planning Board may be allow tandem parking under the following conditions:

- I. Single-family residential projects, multi-family projects, and the residential component of mixed-use projects.
- II. Tandem spaces shall be assigned to the same dwelling unit.
- III. Tandem parking shall not be used to provide guest parking.
- IV. Two parking spaces in tandem shall have a combined minimum dimension of 9 feet in width by 30 feet in length.
- V. Up to 75 percent of the total off-street parking spaces provided may incorporate tandem parking.

6. Loading Standards. If determined necessary by the Planning Board under Site Plan Review, adequate space must be made available on-site for the unloading and loading of goods, materials, items or stock for delivery and shipping. If a loading area is provided it must meet the following standards:

- a. The loading area must be located on the same lot occupied by the use served and must be accessible from a public street or alley.

- b. The loading area must be located to the side or rear of buildings. Loading areas may not be placed between the street and the associated building.
- c. With the exception of areas specifically designated by the Town, loading and unloading activities are not permitted in the public right-of-way.
- d. Loading and unloading activities may not encroach on or interfere with the use of sidewalks, drive aisles, queuing areas and parking areas by vehicles or pedestrians.
- e. The loading dock must be located at least 50 feet away and sufficiently screened from adjacent residential properties.
- f. Each off-street loading space must be designed with a reasonable means of vehicular access from a public street or alley in a manner which will least interfere with traffic movement.

7. Bicycle Parking Facilities. It is the intent of this section is to provide bicycle and pedestrian access to employment, commercial destinations, and other transportation alternatives. Safe and adequate bicycle facilities should be provided that encourage the use of bicycles as an alternative to motor vehicle transportation, thereby reducing traffic congestion, influencing modal split, and increasing the safety and welfare of residents.

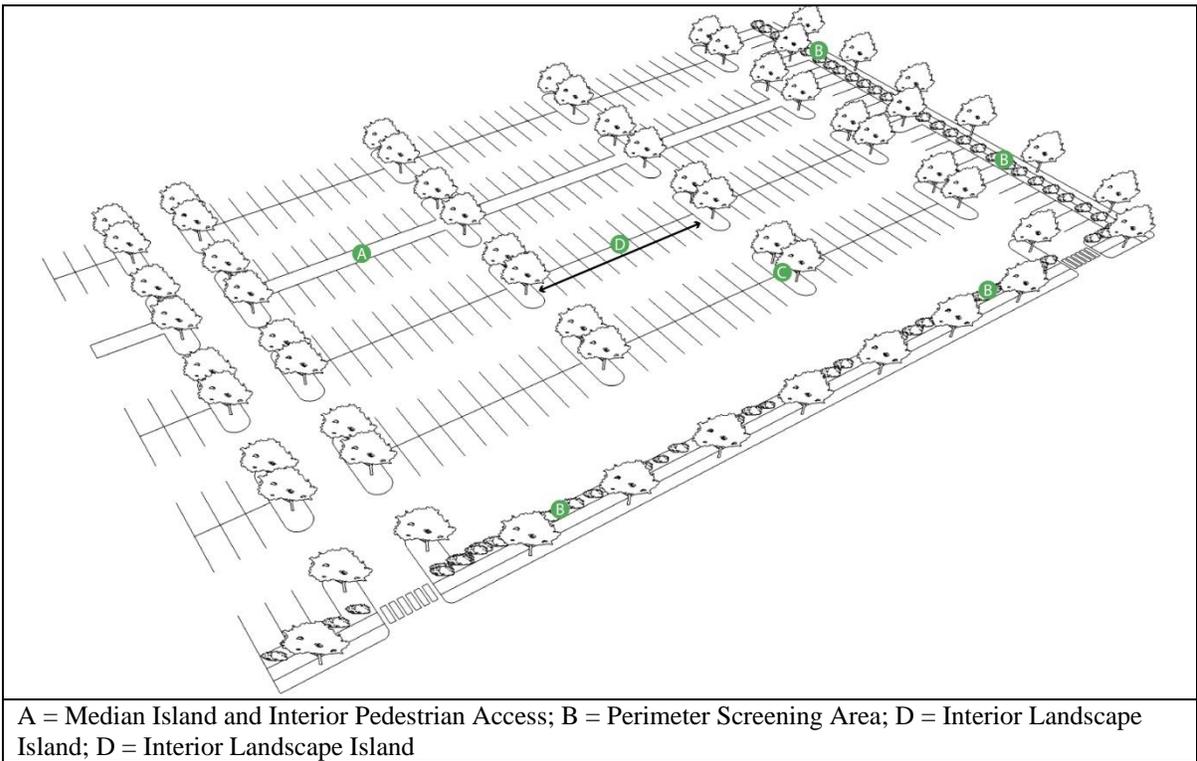
- a. *Multifamily.* All developments with 10 or more multifamily units must provide at least one indoor covered bicycle parking space for every 5 dwelling units unless an alternative is approved by the Building Inspector.
- b. *Nonresidential.* All nonresidential uses must provide the following minimum bicycle parking spaces unless an alternative is approved by the Building Inspector.

Size of Use	Required Spaces
0 to 5,000 square feet of gross floor area	2
5,001 to 10,000 square feet of gross floor area	3
10,001 to 30,000 square feet of gross floor area	4
30,001 square feet or more of gross floor area	6

- I. Required bicycle parking spaces must be publicly accessible and be located no more than 100 feet from the building entrance the bicycle parking space is intended to serve.
- II. Required bicycle parking spaces may be placed on private property or within the public right-of-way. Facilities in the right-of-way must be approved by the Building Inspector, as applicable.

- c. *Bicycle Parking Facilities.* Bicycle parking may be provided through various types of facilities, provided the facility meets the following:
 - I. Each required bicycle parking space is at least 2 feet by 6 feet.
 - II. Bicycle racks are securely anchored, are easily usable with both U-locks and cable locks and support a bicycle at two points of contact to prevent damage to the bicycle wheels and frame.
 - III. Where a bike can be locked on both sides without conflict, each side can be counted as a required space.
 - IV. Spacing of the racks provides clear and maneuverable access.
8. **Parking Structures.** By Special Permit, the ZBA may allow for parking structures as provided for below:
- a. *Parking Garage Requirements.* Parking garages are permitted to the height allowed in the corresponding Zoning District. Parking garages shall be screened from public right-of-ways or, in those instances where screening is not possible, the parking garage shall be given an architecturally articulated street edge with clearly defined openings.
 - b. *Basement or Podium Parking Requirements.* Podium parking may be used in those locations where either topography or density encourages siting parking beneath one or more buildings. Basement or podium parking is intended to have minimal reveal along mapped frontages except at vehicular entry points and shall have either buildings or active uses atop the structure except where circumstances deem such uses unfeasible, as determined by the Building Inspector.
 - c. *Tuck-Under Parking Requirements.* Tuck-Under parking occurs in buildings where enclosed parking is desired underneath a building. Tuck-under parking facilities are not permitted to be visible or accessible from the addressing street and shall be accessed from only an alley or interior portion of a lot.
 - I. Buildings incorporating Tuck-Under Parking shall either by 5 feet from the rear property line or 18 feet but nothing above or in-between.
 - II. Garage entries are not permitted onto any street type other than an alley.
 - III. Garages shall be screened from view along any public street.
9. **Landscaping and Screening.**
- a. *Existing Parking Lot Renovation.* An existing parking lot may be renovated or repaired without providing additional landscaping, provided there is no increase in the size of the parking lot. When an existing parking lot is increased in size landscaping is required for the additional parking area only. When an existing parking lot is increased in size by more than 50% cumulatively, landscaping is required for both the existing parking lot and the new parking area.

- b. Perimeter Screening. All surface parking lots with frontage on any portion of a street right-of-way (not including an alley) must be screened with the following:
- I. A minimum 10-foot wide, landscaped area with a continuous row of shrubs must be provided between the street and parking lot.
 - II. Shrubs must be a minimum of 18 inches in height when planted and must reach a minimum size of 36 inches in height within 3 years of planting.
 - III. A 36-inch wall in a minimum 5-foot planting strip may be substituted for the continuous row of shrubs.
 - IV. Breaks for pedestrian and vehicle access are allowed.



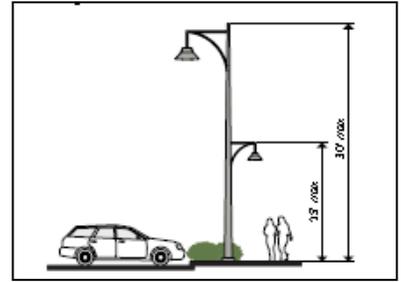
c. Interior Islands.

- I. A landscaped interior island must be provided every 10 parking spaces. Interior islands must be distributed evenly throughout the parking area. Interior islands may be consolidated or intervals may be expanded in order to preserve existing trees.
- II. An interior island abutting a double row of parking spaces must be a minimum of 8.5 feet in width and 300 square feet in area.
- III. An interior island abutting a single row of parking spaces must be a minimum of 8.5 feet in width and 150 square feet in area.
- IV. Interior islands must be installed below the level of the parking lot surface to allow for runoff capture.

- d. Median Islands
- I. A landscape median island must be provided between every 6 single parking rows.
 - II. A landscape median island must be a minimum of 5 feet wide.
 - III. A median island may also serve as the location for a sidewalk. In such case, the sidewalk must be a minimum of 6 feet wide, and the remaining planting area must be no less than 5 feet wide.
 - IV. Median islands may be consolidated or intervals may be expanded in order to preserve existing trees.
 - V. Median islands must be installed below the level of the parking lot surface to allow for runoff capture.
- e. Tree Coverage - Each interior island must include at least one shade tree per 150 square feet. In no case can there be less than one shade tree for every 2,000 square feet of parking area including driving aisles.
- f. Shade Trees - One 3” minimum caliper low water use, low maintenance canopy tree must be provided for every 10. Trees shall be maintained and irrigated as necessary and planted within at least 50 square feet of permeable area. Existing trees located in the interior of lots shall be credited towards this requirement.
- g. Storage Areas - Exposed storage areas, machinery, garbage “dumpsters,” service areas, truck loading areas, utility buildings and structures shall be screened from view of abutting properties and streets using plantings, fences and other methods. Where feasible, shared use and designated areas for garbage dumpsters are encouraged. Trash dumpsters shall be fully screened on 3 sides with solid walls a minimum of six feet high with a solid front gate, six feet high, which shall be kept closed. Trash compacters shall be enclosed to minimize noise.
- h. Coordination - Landscaping of private parking lot and other lot features shall be compatible with the streetscape design elements of the public frontages.
- i. Residential Screening. Any parking, storage or service area which abuts residential districts or uses shall be screened from such residential districts or uses and any parking area shall be screened from a public or private way in accordance with the following requirements:
- I. **Materials.** Plant materials characterized by dense growth which will form an effective year-round screen shall be planted, or a fence or a wall shall be constructed, to form the screen. Where a grill or open-work fence or wall is used it shall be suitable in appearance and materials. Screening may consist of both natural and man-made materials. To the extent practicable, existing

trees shall be retained and used to satisfy the provisions of this Section.

II. **Height.** Screening shall be at least five (5) feet in height. Plant materials when planted, may be not less than 3 1/2 feet in height if of a species or variety which shall attain the required height and width within three (3) years of planting. Height shall be measured from the finished grade.

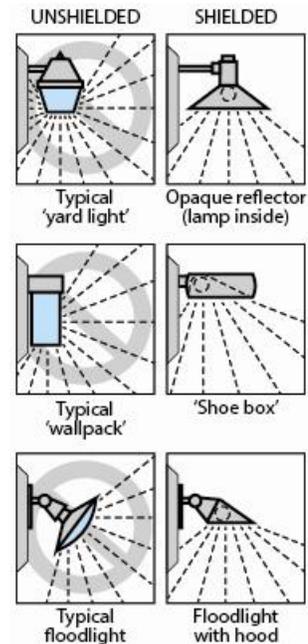


III. **Width.** Screening shall be in a strip of landscaped open space at least five (5) feet wide, and so located as not to conflict with any corner visibility requirements or any other Bylaws of the Town.

j. **Maintenance.** All required plant materials shall be maintained in a healthy condition and whenever necessary replaced with new plant materials to insure continued compliance with screening requirements. All required fences and walls shall be permanently maintained in good repair and presentable appearance and whenever necessary they shall be repaired or replaced.

10. Lighting

a. Lighting elements that cast a perceptively unnatural spectrum of light (such as low pressure sodium and mercury vapor sources) are not permitted. Fluorescent lights, not including compact fluorescent bulbs that use standard sockets, shall not be used for exterior lighting.



b. No flashing, blinking, running, scrolling, traveling, animated, or intermittent lighting shall be visible from the exterior of any building unless associated with a temporary holiday display.

c. Light sources shall be fully shielded or cutoff, and shall not be visible from any street right-of-way or abutting property.

d. Lighting shall use sensor technologies, timers or other means to activate lighting only during times when it will be needed. All non-residential site lighting, including building, sign and parking lot lighting, shall be kept extinguished outside of those business hours, except for lighting determined by the Planning Board to be necessary for site security and the safety of residents, employees and visitors.

e. Parking area light fixtures are limited to a maximum of 30 feet in height. Illumination levels shall be no brighter than what is necessary for the protection and safety of the public in regard to pedestrian and vehicular circulation.

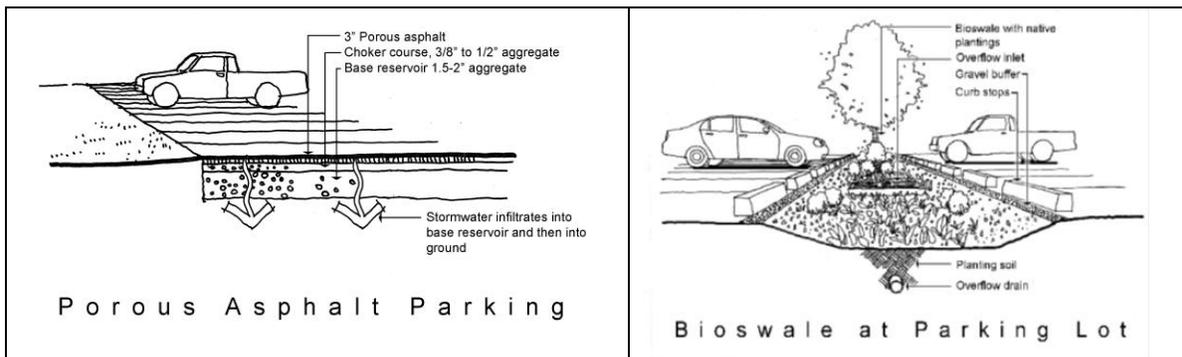
H. CONSTRUCTION.

1. Surface Material. All required parking spaces, maneuvering aisles, and driveways shall have a durable, dustless, all-weather surface, such as bituminous concrete or cement concrete, and shall provide for a satisfactory disposal of surface water by grading and drainage in such a manner that no surface water shall drain onto any public way or onto any lot in other ownership and such surfaces shall be well maintained.
2. Curbing. Parking areas in all Districts shall be provided with curbing, wheel stops, or other devices to prevent motor vehicles from being parked or driven within required setback areas or onto the required landscaped open space.
3. Markings. In any parking area the surface shall be painted, marked or otherwise delineated so that each parking space is apparent.
4. Pervious Parking Materials. As an alternative to Subsection H.1 above, turf grid systems, pervious pavers, gravel and similar parking materials may be allowed for supplemental parking areas where excess parking is necessary on a temporary basis in addition to required off-street parking. Some specific applications may include places of worship, parks and recreation facilities, or public and private schools. Off-street parking facilities surfaced with pervious materials may be allowed by Special Permit from the ZBA as an alternative to impervious materials on required parking areas under the following conditions:
 - a. Driveway aprons from a public street shall be an acceptable impervious material for the first 20 feet;
 - b. Parking surfaces shall be so maintained such that the pervious material does not constitute a nuisance by virtue of its appearance or condition and is graded in a level condition; and
 - c. Selected materials shall comply with the drainage requirements for stormwater runoff set forth in the Town Subdivision Regulations.
5. Drainage
 - a. All surface parking areas must be graded and drained to collect, retain, and infiltrate surface water accumulation on-site to the greatest extent practicable.
 - b. Curbs or parking blocks are required at the edges of perimeter and interior landscaped areas. Curbing must have openings to allow drainage to enter and percolate through

the landscaped area.

6. *Stormwater Best Practices*. Sustainable Design, Low Impact Development (LID), and Light Footprint methods and techniques are encourage in designing and construction surface parking lot as a means to improve the infiltration and quality of stormwater. Some example applications include:

- a. Bioswales/Rain Gardens
- b. Pervious Pavers
- c. Underground Storage
- d. Green Roofs
- e. Rainwater Use for Irrigation
- f. Shared Parking/Stormwater Facilities
- g. Infiltration Parks
- h. Tree Canopy/Ground Cover



I. DEFINITIONS.

See SECTION IA. DEFINITIONS